

NORTH RED LODGE

MASTERPLAN DOCUMENT

FINAL

DECEMBER 2022

eclipse
PLANNING SERVICES

BARTON
WILLMORE now  Stantec

Issue Date	December 2022
Job Number	28730
Document Status	FINAL
Revision	R
Authors	KS/DS/AC
Checked By	OC - AC
Authorised By	DS

CONTENTS

INTRODUCTION:	4	FRAMEWORK PLAN:	22
PURPOSE OF DOCUMENT		- MASTERPLAN DESIGN PRINCIPLES	24
PLANNING AND POLICY CONTEXT		SUSTAINABLE DESIGN PRINCIPLES	
VISION	6	KEY SPACES AND CHARACTER AREAS	
VISION FOR NORTH RED LODGE		GENERAL CAR PARKING PRINCIPLES	
SITE AND CONTEXT:	8	GENERAL RESIDENTIAL BLOCK PRINCIPLES	
SITE LOCATION, CONTEXT AND MORPHOLOGY		- TECHNICAL DETAILS:	40
SURROUNDING LAND USES AND COMMUNITIES		PARAMETERS: LAND USE AND INFRASTRUCTURE	
LANDSCAPE CONTEXT AND CHARACTER		PARAMETERS: RESIDENTIAL DENSITY	
ECOLOGY CONTEXT		PARAMETERS: BUILDING HEIGHTS	
CONSTRAINTS AND OPPORTUNITIES		EMPLOYMENT	
COMMUNITY ENGAGEMENT		ACCESS AND MOVEMENT	
		BLUE INFRASTRUCTURE	
		LANDSCAPE AND ECOLOGY:	62
		OTHER POTENTIAL BENEFITS:	72
		IMPLEMENTATION FRAMEWORK	74

INTRODUCTION

PURPOSE OF THE DOCUMENT:

Land to the north of Red Lodge is allocated for a mixed use development in the Forest Heath area of West Suffolk Council's Site Allocations Local Plan (SALP) (September 2019) under Policy SA10. Requirement A of that policy states that planning applications will only be determined once a masterplan for the whole site has been approved by the Local Planning Authority.

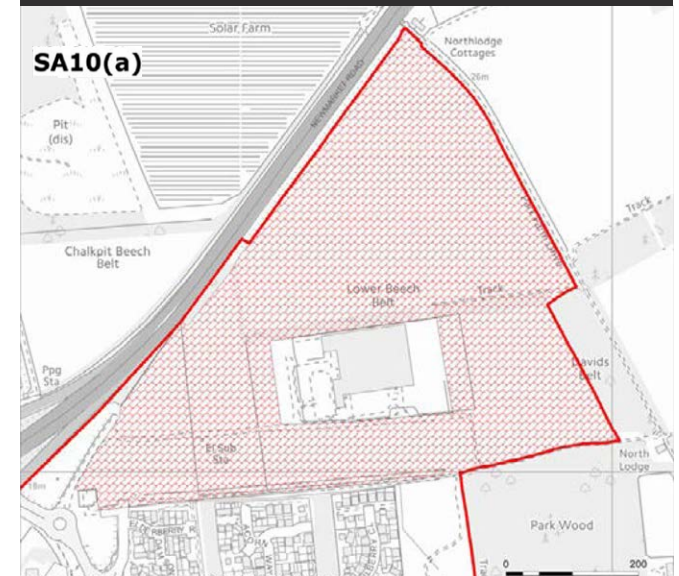
The purpose of this masterplan is to set out clear guidance for developers and to provide a development framework, based on a full understanding of the allocated site to the north of Red Lodge and its context, against which West Suffolk Council (the Council) will assess future planning applications. The masterplan will be a material consideration in the determination of future planning applications.

It should however be a flexible document that can evolve as the development progresses subject to the agreement of the Council.

This document sets out the vision for a new neighbourhood to the north of Red Lodge. It:

- Sets a framework masterplan, identifying distinctive character areas;
- Outlines how the design should respond to the site context, constraints and characteristics of Red Lodge;
- Describes the movement and access principles in and around the site;
- Sets out the strategic landscaping and open space proposals;
- Explains the measures proposed to safeguard the integrity of the Breckland Special Protection Area (SPA); and
- Explains how the development should address the specific requirements of SALP Policy SA10.

FIGURE 1 SALP SITE ALLOCATION



PLANNING AND POLICY CONTEXT

The National Planning Policy Framework (NPPF) (2021) clarifies that “the creation of high quality, beautiful and sustainable buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development more acceptable to communities.” It states that being clear about design expectations, and how these will be tested, is essential for achieving this.

Planning Practice Guidance sets out that “Good design responds in a practical and creative way to both the function and identity of a place. It puts land, water, drainage, energy, community, economic, infrastructure and other such resources to the best possible use – over the long as well as the short term.”

GOOD DESIGN IS A KEY ASPECT OF SUSTAINABLE DEVELOPMENT, CREATES BETTER PLACES IN WHICH TO LIVE AND WORK AND HELPS MAKE DEVELOPMENT MORE ACCEPTABLE TO COMMUNITIES.

Policy DM3 of the Council’s Joint Development Management Policies Document (2015) states that masterplans will be required for sites allocated in Local Plans and Site Allocations DPD.

Policy SA10 of the SALP sets out the focus of growth to the north of Red Lodge and proposes a mixed use development comprising:

- 300 dwellings;
- 8 hectares of employment land;
- 3 hectares for a new primary school; and
- Other appropriate uses.

Relevant policies contained within the NPPF, Forest Heath Core Strategy, the Joint Development Management Policies Document, the Single Issue Review (SIR) and the SALP for the Forest Heath area of West Suffolk Council have informed the preparation of this masterplan.

VISION

VISION FOR NORTH RED LODGE

The vision is to create a new sustainable neighbourhood to the north of Red Lodge set within an attractive environment that responds to the local character and landscape setting of Suffolk.

The new development should fully integrate existing and new residential areas, with local services, new employment and open space, delivering benefits to the existing community as well as the new residents. The new primary school known as The Pines, has already been provided and opened in September 2018. When the rest of the North Red Lodge development is complete it will provide around 300 high quality homes of mixed tenure, a new local centre, major employment opportunities, further scope for the primary school to expand to meet local need and new areas of open space.

The development will have its own distinct qualities, whilst responding to local landscape features, the site context and ecological constraints. Retained mature trees will provide opportunities for linear parks which will form a strong green network providing easy access to housing and encouraging healthier lifestyles.

Opportunities will be taken to reduce carbon emissions and create climate change resilience.

The importance of biodiversity protection and enhancement will be recognised and integrated in to the design process to create attractive natural green spaces.



The North Red Lodge development will deliver:

Around 300 new homes.

A high-quality public realm to create an attractive and welcoming neighbourhood.

Footpath and cycle connections within the site which link to the existing network providing access to a range of facilities.

8 ha of new employment that is well integrated with the residential areas.

A new Local Centre.

Opportunities for the Pines Primary School to expand.

A well linked development with clear connections and movement network

- The setting of the new development provides opportunities to enhance and incorporate the existing pedestrian and cycleways.
- Provide direct, safe and attractive routes to the new employment areas, local centre, primary school and residential areas, as well as to the existing facilities in Red Lodge for both new and existing residents.
- Create a footpath network that could be used for dog walking and connect into the perimeter recreational footpath route around Red Lodge.

A new community hub with high quality public realm

- Create a new local centre or “community hub” that can be the focal point for community activity.
- Provision of a new local centre which is distinctive and vibrant supporting the need for new and existing populations.
- Opportunities to facilitate a range of non-residential uses, which could include local shops, restaurants, takeaway, small scale commercial uses, a community building and /or health facility.
- Incorporate the School drop-off area within the hub.

A development set within a strong green network

- New landscape planting to comprise locally native species.
- Creation of linear parks retaining mature trees and tree belts .
- Opportunities to provide a strong network of green corridors that offer access to formal and informal open spaces for both new and existing residents.
- Enhance and protect the existing habitats to encourage both flora and fauna within the built environment.
- Opportunity for new areas of informal/ semi-natural grassland to maintain the existing interest of the site.
- Creation of Suitable Alternative Natural Greenspace (SANG) well connected to the network of green spaces.

A new development providing new homes, employment and education

- Enhance the vitality of the existing community (by providing of a range of homes including affordable housing), making it a place that is attractive to live and work.
- SALP Policy SA10(J) requires the provision of 8ha for a mix of employment uses and 300 homes.
- SALP Policy SA10(J) also requires the provision of 3 hectares for a new primary school. The Pines Primary School is open and land will be safeguarded to enable its future expansion.



RESIDENTS ENJOY ACTIVE LIFESTYLES



NEW DEVELOPMENT CAN SUPPORT FACILITIES IN THE WIDER AREA



FLEXIBLE INTERNAL LAYOUTS REQUIRED TO MET CHANGING NEEDS



RESIDENTS WILL ENJOY BEING WITHIN CLOSE PROXIMITY OF NATURE



RESIDENTS TYPICALLY COMPRISE A MIXTURE OF FAMILIES AND THOSE IN EARLY RETIREMENT

SITE AND CONTEXT

SITE LOCATION, CONTEXT AND MORPHOLOGY

Red Lodge is a settlement located on the south eastern side of the A11 Newmarket Road, approximately 4.5 kilometres (3 miles) to the South of Mildenhall and 8 kilometres (5 miles) to the North East of Newmarket. It lies within the administrative area of West Suffolk Council (and prior to April 2019 Forest Heath District Council).

The allocated site at North Red Lodge lies to the north of the settlement of Red Lodge and comprises circa 28 hectares (69 acres).

It is a triangular shape and comprises low grade, predominantly Grade 4, agricultural land and The Pines Primary School.

The allocated site is bound to the west by the A11 to the south by residential development, an allocation site known as SA9(b) Land East of Red Lodge: North and Park Wood. Open fields and three Estate farm cottages to the north east beyond Park Farm Drive and one to the south east.

The site allocation wraps around Hamelin's office and warehouse which is located in the middle of the new development area. A small area located to the north and west of Hamelin's primary access sits outside the allocated site. As it is located within the settlement boundary it has been included within the masterplan boundary to enable a comprehensive development for the area to be brought forward. Conversely, Hamelin's tree belt immediately north of the warehouse and its eastern SUDs ditch are included within the SALP site allocation boundary. As these are within separate ownership, they have been excluded from masterplan.

The site slopes down gently from north east to south west, falling from approximately 24m Above Ordnance Datum (AOD) to 18m AOD respectively.

The vegetation on the site is mainly located along the boundaries or within tree lines and shelter belts, dividing the site into smaller parcels.

A high-pressure gas pipeline passes East to West through the site.

Figure 2 shows the location of the site in relation to the wider area and road network.

Red Lodge is not a typical Suffolk village, as it does not have a recognisable historic or central core, having developed along the A11 after the Second World War.

The townscape of Red Lodge consists of a wide mix of housing styles, ages and types, with a large component of 21st century development.

The initial development of Red Lodge occurred over 10 years from the early 1920s, along Turnpike Road. Due to the continual cycle of individual plots being demolished or altered, the dwellings here are predominately single storey located within generous plots, set back from the road side with deep enclosed front gardens.

Red Lodge experienced further development in the mid-20th century which filled the remaining land between Newmarket Road and Warren Road. This development is largely formed by a number of small cul-de-sacs served off Heatherset Way which loop to and from Warren Road. This area of development typically contains similar dwelling types grouped together to create rather uniform street scenes.

Recent residential development can be found along Turnpike Road to the west and within the Kings Warren development to the east. Their key design characteristics include: varied typology, buildings set back from the main road, retention of existing vegetation, typically belts of

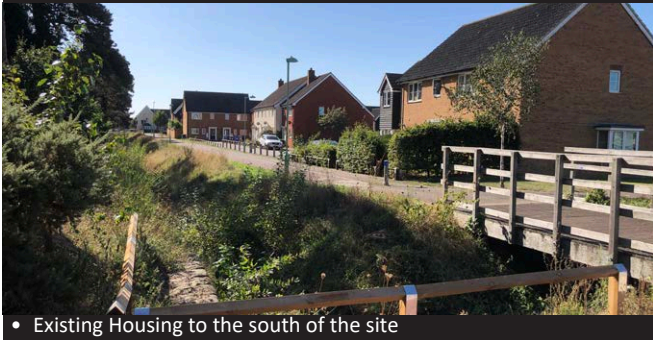
pine trees. The built environment of the settlement is characterised by an eclectic mix of building styles, including a variety of brick colours, some examples of black weather boarding, flint and rendering.

The new Kings Warren development to the east of Red Lodge combines a mix of materials and detailing that reflects the West Suffolk vernacular with simple, modern design. A perimeter block form creates a strong and well defined public realm, guiding residents and visitors through the streets; back-to-back and back-to-side relationships are carefully considered, particularly where phases meet; tree belts and small areas of public open space create an openness in the street scene for the main routes, contrasting with a more urban form in the shared surface streets and mews areas.

The masterplan aims to reflect the existing context, as well as draw on the architectural characteristics of Suffolk villages to create a new development which has a positive sense of place and integrates well with Red Lodge.



- Existing school drop off - pick up



- Existing Housing to the south of the site



- David's Belt



- Existing Footpath - Located to the south of the site

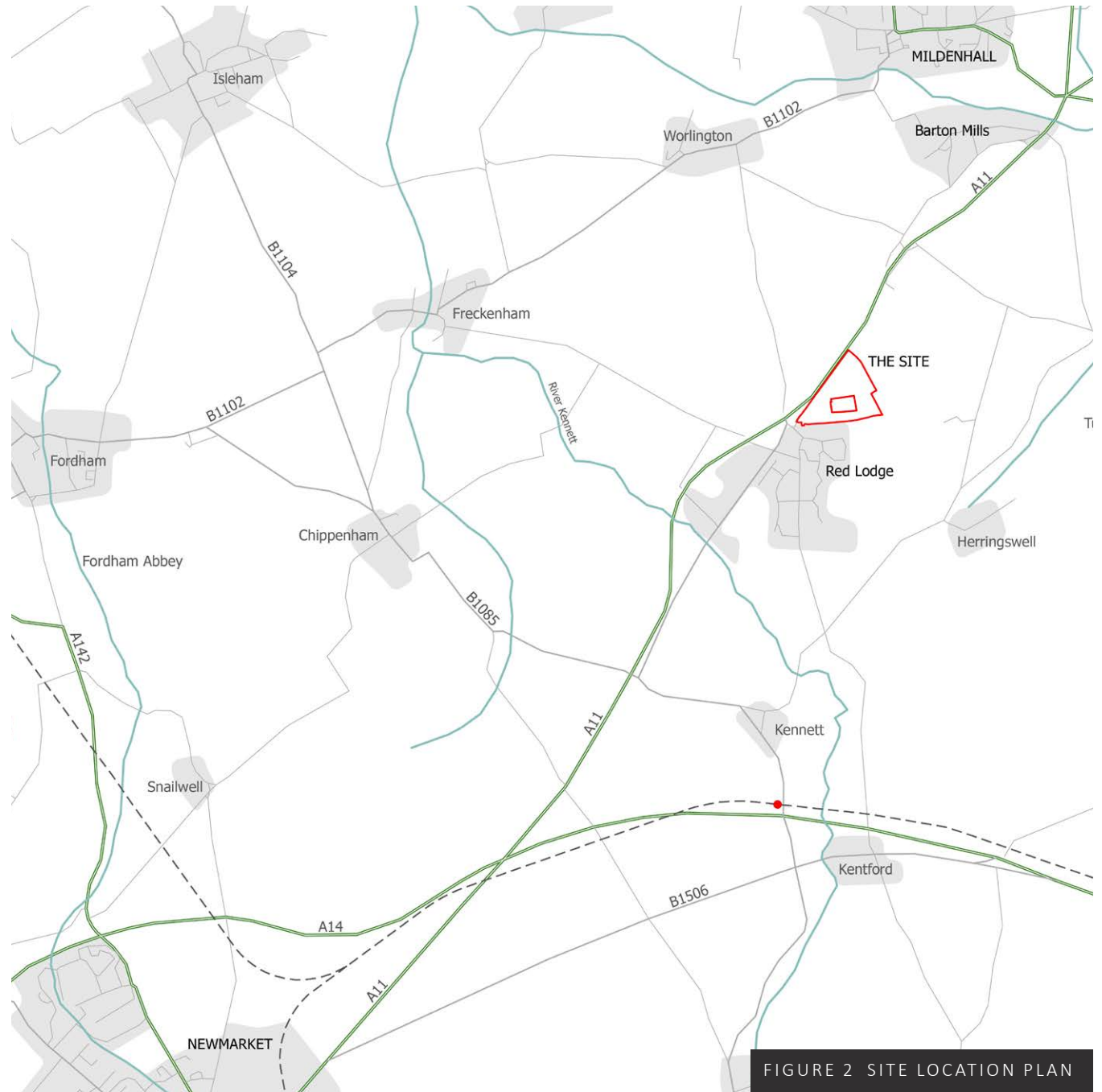


FIGURE 2 SITE LOCATION PLAN

SURROUNDING LAND USES AND COMMUNITIES

Until Forest Heath District Council prepared its 1995 Local Plan, Red Lodge was characterised by developments related to roadside services, local mineral operations and a large area of low-density housing.

Red Lodge was identified as a focal point for growth in the 1995 Local Plan, with an allocation for approximately 1500 new homes, a primary school, village centre with health and retail facilities and new employment opportunities. Land to the north of Red Lodge was identified for a mix of business and industrial uses. In 2003 planning permission was granted for a mixed-use development including new homes, services, commercial uses and open space. The residential development, known by its sales name as “Kings Warren”, is now complete. Construction of the next phase of development for 374 homes on adjoining land to the east, is advanced.

In addition, new residential developments have built along Turnpike Road to the west of Red Lodge.

Community facilities include an Ecumenical Church, Village Hall and venue, known as the Millennium Centre, a sports pavilion with tennis courts, a five-a-side football pitch, cricket pitch, allotments and village centre. St Christopher’s Primary School opened in September 2012 and The Pines Primary School, which falls within the site allocation, opened in September 2018.

Bounding the site to the west is the A11 with agricultural land, a mineral extraction site and a solar farm beyond.

Park Drive and three Estate cottages border the site to the north and south east, with agricultural land beyond. A woodland belt known as Park Wood adjoins the site to the south east.

Hamelin Brands Limited is located right in the middle of the new development. Immediately to the south of Hamelin, there is the Pines Primary School.



• Hamelin Brands Limited



• Kings Warren development

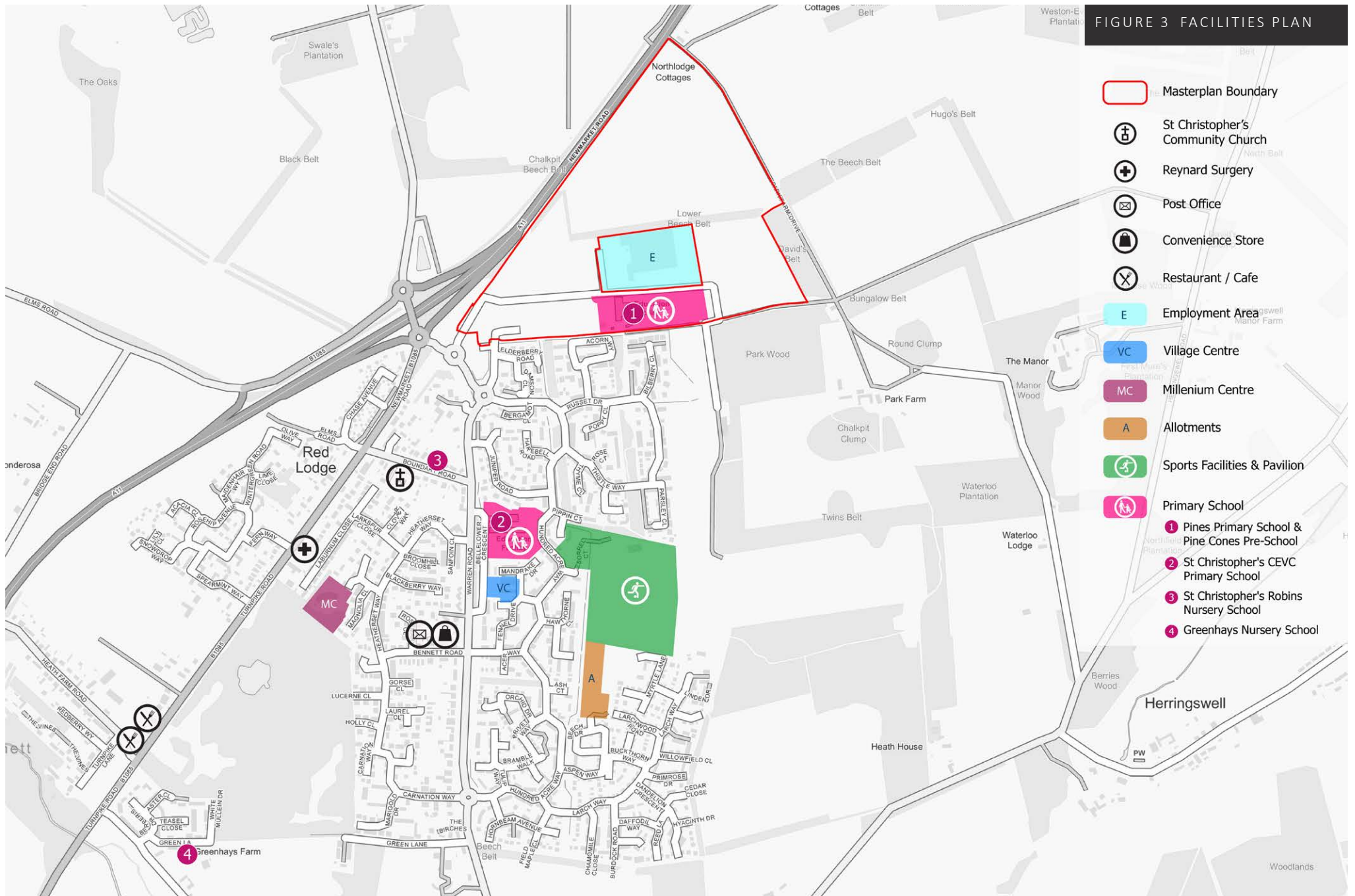


• Kings Warren development



• The Pines Primary School

FIGURE 3 FACILITIES PLAN



- Masterplan Boundary
- St Christopher's Community Church
- Reynard Surgery
- Post Office
- Convenience Store
- Restaurant / Cafe
- Employment Area
- Village Centre
- Millenium Centre
- Allotments
- Sports Facilities & Pavilion
- Primary School
 - 1 Pines Primary School & Pine Cones Pre-School
 - 2 St Christopher's CEVC Primary School
 - 3 St Christopher's Robins Nursery School
 - 4 Greenhays Nursery School

LANDSCAPE CONTEXT AND CHARACTER

The allocated site is not covered by any statutory or non-statutory landscape designations.

Some trees are protected. The two individual pine lines running north to south through the western part of the site and the line of trees on the western and southern boundaries are protected by TPO No. 3/1994.

The site lies towards the southern extent of the Breckland area that straddles the county boundary of Norfolk and Suffolk and a very small part of Cambridgeshire. The Brecklands are an area of distinctive landscape formed by the combining of soil type, climate, land-use history and wildlife. The site falls within the National Character Areas (NCA) for The Brecks (NCA 85) which stretches from Red Lodge and Mildenhall in the south west to Swaffham in the north. It is an area characterised by the predominance of agricultural land, amongst other features.

The 2008 Suffolk County Council Landscape Character Assessment identifies the site as Estate Sandlands (Type 7) which is found in the northeast and centre of Forest Heath and characteristics include:

- Strongly geometric structure of fields enclosed in the 18th and 19th century;
- Widespread planting of tree belts and rectilinear plantations;
- Characteristic 'pine lines'.

The existing site comprises ploughed fields, field edge scrub, tree belts, an existing access road and primary school. The land cover and land use is predominantly agricultural in character, albeit strongly influenced by the urban edge.

The site and its surrounds have an urban fringe character. It is enclosed to the north and west and east by a heavy tree and shrub belt. The southern boundary is enclosed by existing residential development and the SALP site SA9(b) Land East of Red Lodge: North.

The landscape character of the site includes some of the features described in Suffolk County Council's Landscape Type 7: Estate Sandlands, most notably the belts of Scots pine which compartmentalise part of the site. However, it is generally more physically enclosed compared to the majority of the character area. Its character is also influenced by the immediately adjacent housing to the south, the noise and structural presence of the adjacent A11 road, which appears in general to cut across the landscape in a discordant nature. The character of the site is influenced by the imposing nature of the existing warehouse and the associated lorry movements along the access road, combined with the estate roads with associated lighting, and the 'business park' feel to the centre of the site. Overall the site is not strongly reflective of the identified characteristic features for the area.

The flat nature of the site limits the potential visual impacts in views from the surrounding landscape with the perimeter tree belts providing a visual buffer to its surroundings. Views into the site are near views, the majority of which are from the neighbouring urban fringe residential area.

The Landscape and Visual Impact Assessment accompanying a planning application will take account of the national and County based landscape assessments, together with any other relevant studies and local plan policies as necessary.

***STRONG GEOMETRIC
STRUCTURE OF FIELDS
ENCLOSED IN THE 18TH
AND 19TH CENTURY***

FIGURE 4 LANDSCAPE CHARACTER PLAN



ECOLOGY CONTEXT

Ecological Designations

The allocated site is not subject to any statutory or non-statutory ecological designations. The nearest statutory designation is Red Lodge Heath Site of Special Scientific Interest (SSSI) approximately 0.7km south-west of the site. The nearest non-statutory designation is Worlington Chalk Pit County Wildlife Site (CWS) located approximately 0.15km to the west of the site on the opposite side of the A11.

The nearest European conservation designation is Breckland SPA located 1.4km to the south-east of the site boundary at its nearest point. The SPA is designated for its breeding populations of Stone Curlew, Woodlark and Nightjar. The south eastern tip of Site SA10 (a) lies within the 1500m SPA Stone Curlew buffer, as shown on Figure 5.

The Habitat Regulation Assessment (HRA) of the SALP undertaken for Forest Heath District, reviewed the impacts of the north of Red Lodge allocation and ruled out the potential for adverse effects on the integrity of Breckland SPA. However, this was on the basis of measures being in place to mitigate against the potential for recreational effects on Breckland SPA, and Breckland Forest SSSI located less than 3km to the northeast of the site.

Ecology Survey Works

An ecological desktop study, Phase 1 Habitat Survey was undertaken by Aspect Ecology during 2016. A general appraisal of faunal species to record the potential presence of any protected, rare or notable species. Following this appraisal, detailed surveys were undertaken in 2016 for breeding birds, badger, bats and reptiles. A targeted arable flora survey was also undertaken. The Phase 1 Habitat Survey was subsequently updated by Aspect Ecology in August 2018, and September 2020.

Update surveys for bats and badger and reptiles were also carried out in September 2020. The ecological impacts of the proposals will be assessed as part of any planning application.

Habitats

The site is dominated by arable land, with woodland belts and tree lines, grassland, tall ruderal vegetation and scrub.

The woodlands within and adjoining the allocated site are largely plantations. Lower Beech Belt (to the north of the Hamlins building) and Davids Belt to the east, qualify as UK Priority Habitat, but are not considered to be of high quality, due to the restricted structure and diversity of flora. The treelines and scattered trees within the site offer ecological connections across the site. Overall the woodlands and scattered trees are considered to be of moderate ecological value at the local level. As David's Belt and a proportion of Lower Beech Belt is mapped as deciduous woodland Priority Habitat, the woodland and

scattered trees within the site are considered to be an important ecological feature.

Other habitats, arable flora, semi-improved grassland and scrub, support locally common and widespread species and do not form important ecological features and are considered to be of no more than local level importance.

THE SITE IS DOMINATED BY ARABLE LAND, WITH WOODLAND BELTS AND TREE LINES, GRASSLAND, TALL RUDERAL VEGETATION AND SCRUB.

Fauna

The site is utilised to some degree by an assemblage of eight bat species including Annex 1 species Barbastelle, and three UK Priority bat species which use the application site for commuting and foraging. A small number of trees are present with low to moderate bat roosting potential. The site is considered to be of value to commuting and foraging bats. Surveys have recorded moderate levels of activity within the site, which focussed on the existing boundary features such as the tree lines. No activity was recorded along the north western boundary adjacent to the A11.

Badger is known to be present within the wider area, but no evidence of presence was found within the site.

In addition, other common mammal species are present, including the Priority Species Brown Hare.

The 2016 and 2020 surveys recorded no evidence of the presence of reptiles, although it did note a modest assemblage of 20 breeding bird species, with a small number of Red-listed farmland species such as Yellowhammer.

Overall the site supports fauna populations of low ecological value at the site or local level. Due to the bat species recorded, the site is considered to be of moderate value at the district level for commuting/foraging bats.



• existing trees within the site



• existing trees within the site

FIGURE 5 ECOLOGY PLAN



CONSTRAINTS AND OPPORTUNITIES

Constraints includes:

- Noise and severance from A11
- HSE major hazard pipeline
- Recreational pressure and SPA constraint zone
- Impact on landscape and ecology
- Public transport accessibility
- Interface between employment and residential

Opportunities includes:

- Connectivity to wider active travel networks in Red Lodge
- Green infrastructure - protection of pine lines and retention/enhancement of the existing tree belts and adjacent woodland habitats through connection to the wider landscape
- Creation of Community Hub
- New public open space / SANG
- Local employment opportunities
- Access to local community facilities and services

Noise

The A11 borders the site to the west and is the dominant continuous noise source affecting the site. SALP Policy SA10(E) requires the provision of noise attenuation measures as part of any development. A Noise Impact Assessment has found that other potential noise sources affecting the new residential areas within the masterplan site are the primary school, village centre uses, the new and existing employment areas.

Key design considerations:

- Avoid placing sensitive land uses into areas with an unacceptable level of noise.
- Local centre: needs careful design to safeguard residential amenity against noise pollution. Consideration to be given to the use of acoustic barriers, and glazing specifications for the new dwellings;
- New employment areas: locate adjacent to the A11 in order to reduce their potential noise impact. New residential areas will be well inset into the site and separated from the employment areas by open space. Consider how commercial buildings can also act as an acoustic barrier to traffic noise from the A11 in place of an acoustic screen.
- New employment north of the existing spine road (west of Hamelins): protect the amenity of residents to the south of the spine road from noise and disturbance through careful design. No vehicular access or outside activity should take place south of the employment buildings close to the spine road.
- Acoustic barrier to be provided along the A11. Existing bund to be retained, extended with acoustic screen and both should be landscaped.
- Naturalised mounding to be provided within open space between the employment area and new residential development to attenuate A11 traffic noise.

HSE major hazard pipeline

SALP Policy SA10(H) states that the advice of HSE must be adhered to regarding development near the major hazard pipeline.

A high pressure gas pipeline crosses the site and presents a degree

of constraint to the type and form of development proposed within the 78m “inner and middle” HSE consultation zones. Regard must be had to the HSE’s Land Use and Planning Methodology guidance. This, together with the HSE’s web app, uses a decision matrix and will provide a response based on data input. Where a mixed use development is proposed, the Council and developers should seek bespoke advice from HSE as necessary.

Discussions with HSE have agreed the following:

- No dwellings are permitted within the inner and middle zone;
- The location of the employment area within the HSE inner zone is acceptable providing that the buildings are no more than two storey and accommodate less than 100 people;
- The provision of a local centre within the outer zone is acceptable;
- Landscape buffer: the provision of public open space between the northern employment area and new dwellings is acceptable as long as it’s design does not encourage public gatherings. Footpaths/cycleways, the occasional bench and landscaping are permitted, but not a formal play area, events area, picnic benches or marquees.
- HSE are not concerned about services or local roads located within the inner zone as this is a matter for the network operator.

Any consultation with HSE on the masterplan and subsequent planning applications should be on the basis of its bespoke advisory service.

Discussions with National Grid have confirmed:

- New road crossings need to be perpendicular to the pipeline with appropriate protection; and
- New planting will need to comply with National Grid’s guidance for tree planting restrictions on pipelines.

FIGURE 6 CONSTRAINTS AND OPPORTUNITIES PLAN



Landscape

The key landscape features within and adjacent to the site are the protected trees on the western and southern boundaries, the two pine lines that run north-south through the site framing and compartmentalise the site and the trees on the eastern boundary of Davids Belt and Park Wood.

To the north of the Hamlins buildings is a tree line running east west known as Lower Beech Belt. That section of the tree belt that is immediately adjacent to the Hamelin site is not within the ownership of the Park Farm Estate (the Landowner for the wider masterplan area).

Existing trees and vegetation should be maintained and enhanced where possible and development of the site can offer the opportunity to provide significant landscape improvements to an area that currently does not represent a high value for recreation or habitat and does not contribute to the sense of place. Where the arable land separates the woodland belts and semi-improved grassland, limiting habitat connectivity and lack of public accessibility, new planting and green spaces can provide these connections and allow access with recreational opportunities.

Key design considerations:

- Opportunity to provide significant landscape improvements to an area that does not have a high recreational or habitat value.
- Opportunity to create two distinct linear parks, new play area and linked open spaces that are safe, welcoming and accessible.
- Retain and enhance existing pine lines and tree belts.
- Opportunity for street tree planting.
- Provision of a landscape buffer along the A11.
- Opportunity to provide a large area of well connected, accessible open space which can function as a SANG.

Ecology

The site is primarily arable, with woodland belts, tree lines, semi-improved grassland and scrub. The trees and woodland offer ecological connections across the site. There is evidence of bats using the site for foraging and commuting and some breeding birds.

Those features of greatest ecological value will be retained, although some habitat loss will occur. This is unlikely to be of significance and will be compensated for by the creation of green space and landscape planting.

The potential effects on fauna during construction and operational phases of the development will be mitigated.

Key design considerations:

- Opportunity for new areas of informal/semi-natural grassland to maintain the existing interest of the site;
- New landscape planting to comprise locally native species.
- Opportunities for bird and bat boxes and new invertebrate habitats. At least 10% of all dwellings should have integrated bird or bat boxes.
- Protect retained trees. Any lighting scheme should create dark corridors for wildlife.
- Opportunities for new public open space and SANG. Consider the location of greenspaces away from the A11 and its traffic noise.
- Provision and promotion of dog walking routes, including circular walks, larger areas of linked greenspace.
- Provision of barriers to access of adjacent farmland as required by Policy SA10 (B).
- Delivery of avoidance, mitigation, compensation and enhancements for fauna known to be present at the site (for example provision of hop-overs where tree removal is required for access road to safeguard commuting and foraging bats).
- The small area of land within the 1.5km of Breckland SPA will be greenspace only.



• Existing footpath/cycle path to south of the site



• Trees to be retained



• Existing green corridors with mature trees

Recreational pressure and SPA constraint zone

SALP Policy SA10(B) states:

“B) The masterplan and any future planning applications will require a project level Habitats Regulations Assessment. The development must also provide measures for influencing recreation in the surrounding area, to avoid a damaging increase in visitors to Breckland SPA and an increase in recreational activity in adjacent farmland. Measures should include the provision of suitable alternative natural greenspace which is well connected and the enhancement, and promotion of dog friendly access routes in the immediate vicinity of the development, barriers to access and/or other agreed measures.

The developer is required to submit information that clearly demonstrates that the above measures would result in no adverse effects on the integrity of Breckland SPA. This information will include:

- *details of the timetable for implementation of all measures;*
- *availability of measures at the time of occupation of the new dwellings – including any phasing plan if applicable;*
- *details of adoption and future management of measures (as required);*
- *a concept design for the SANGS.*

Planning permission will not be granted unless this information is sufficient to allow the local planning authority (as competent authority) to conclude that the requirements of the Habitats Regulations 2017 (or any replacement regulations) are satisfied.”

The masterplan has been developed to capture the requirements in relation to the delivery of a SANG which will function to deflect visitors away from Breckland SPA and therefore ensure the proposals will not affect the integrity of the designation.

Timescales for the SANG concept design and details of the SANG key principles are contained in the Landscape and Ecology Section of the masterplan (pg 68).

A Document to Inform a Habitats Regulations Assessment has been prepared to assess whether the masterplan would generate likely significant effects on European designations. The assessment found that in the absence of any mitigation, likely significant effects could not be ruled out in relation to increases in recreational pressure at Breckland SPA, and therefore an Appropriate Assessment was carried out. This assessed whether the masterplan delivers/is capable of delivering the mitigation measures set out in SALP Policy SA10(B) (such as barriers to access) and other documents (such as a Accessible Natural Greenspace Study). The assessment found that the masterplan does/can deliver all of the measures set out, and with these measures in place concluded there would be no adverse effect on the integrity of the SPA as a result. A number of the mitigation measures are already incorporated into the masterplan (such as provision of a SANG), whilst other would be specified in full at the detailed design stage.

A project level Habitats Regulations Assessment will accompany the masterplan and future planning applications.

Footpaths and Cycleways

Red Lodge has a good network of footpaths and cycleways and these will be extended into and round the masterplan site in accordance with SALP Policy SA10(I).

Key design considerations:

- Opportunities to enhance pedestrian and cycleways providing direct, safe and attractive routes to the new employment areas, local centre, primary school and residential areas, as well as to the existing facilities in Red Lodge.
- Provision of circular dog walking routes that would connect into the perimeter recreation loop that is being provided to the east of Red Lodge.

Contaminated Land

The site is undeveloped and the risk to future users is likely to be low, however a Geotechnical Phase 1 Desk Study will be submitted with any planning application to assess this matter.

Public Transport Accessibility

Bus

The nearest timetabled bus stops to the site are the 'Adjacent to Thistle Way' stop located on the eastern side of the carriageway and the 'Opposite Thistle Way' stop located directly opposite. These stops lie approximately 400m south west of the site on Hundred Acre Way.

Rail

Kennet Rail Station is approximately 3.75km to the south of Red Lodge, with daily services between Ipswich and Cambridge. Whilst it exceeds the 2km distance generally regarded as the maximum acceptable distance for accessing rail travel on foot, multimodal journeys are feasible by bus and cycle.

Key design considerations:

- Opportunity to design the road network for future bus access.

Sustainable Drainage

There are no watercourses within the site and it falls entirely within Flood Zone 1 and has a low probability of flooding. Infiltration testing has established that the site drains well and that there are opportunities for sustainable urban drainage.

Historic Environment and Archaeology

SALP Policy SA10 (F) requires an archaeological evaluation to be carried out prior to decisions on the site layout and determination to allow preservation in situ where appropriate and to allow appropriate archaeological strategies to be defined.

There are no listed buildings or Scheduled Ancient Monuments within or adjoining the site.

In March 2016 a geophysical survey of the site was undertaken. It confirms that no anomalies of an archaeological or probable archaeological origin were identified. On the basis of this survey and the findings of other archaeological investigations undertaken within the vicinity, Suffolk County Council Archaeological Service has confirmed that trial trenched archaeological evaluation and any mitigation, can be secured by planning condition rather than undertaken prior to a decision.

New Employment

In accordance with SALP Policy SA10(J) 8 hectares of employment land will be provided. A mix of business (formerly Class B1, now Class E(g)), general industry (Class B2) and storage and distribution (Class B8) uses are permitted by SALP Policy SA17 (employment allocations). The exact mix will be the subject of future planning applications.



• Existing footpath/cycle path to south of the site



• Existing SUD's



• Existing SUD's

Local Centre

SALP Policy SA10(J) allows for the provision of other appropriate uses. A new local centre or “community hub” will act as a focal point for community activity and contain some apartments and opportunities for a range of local facilities.

The new local centre will be well served by pedestrian and cycle links and accessible to both new and existing residents on foot, or cycle as well as by car.

New Primary School

SALP Policy SA10(J) requires the provision of 3 hectares for a new primary school. In 2017 planning permission was granted for a 420 place primary school and a 30 place pre-school within the masterplan site. The Pines Primary School and Pine Cones Nursery opened in September 2018. Provision will be made for The Pines to expand to meet this requirement of planned growth at Red Lodge.

COMMUNITY ENGAGEMENT

Consultation on key issues relating to the development of this site has been undertaken with stakeholders and the local community and this has informed the development of the draft masterplan Framework.

Planning History

In October 2016 an planning application was submitted to Forest Heath District Council for the construction of up to 46 dwellings, buildings for employment use (Class B1, B2 and B8) and associated access, parking, landscaping, open space and infrastructure works, on part of the masterplan site (planning reference DC/16/2364/OUT). It was withdrawn before it was determined to allow for a masterplan to be prepared for the wider site that is now allocated. Prior to submission of the planning application, the applicant, consulted key stakeholders including Forest Heath District Council, Suffolk County Council, Environment Agency, Anglian Water, Ward Members and Red Lodge Parish Council.

A public exhibition was held in July 2016 at the Red Lodge Sports Pavilion and was brought to the attention of consultees and local interest groups using postal invitations, posters displayed locally and an advert in the local newspaper. It was held together with Suffolk County Council and Concertus Design and Property Consultants who presented proposals for The Pines Primary School.

Masterplan

Work commenced on the preparation of this masterplan in Spring 2018. A series of meetings and discussions have since taken place with officers at West Suffolk Council and Suffolk County Council.

A meeting with the Health and Safety Executive (HSE) and discussions with National Grid have clarified their respective development requirements in respect of the gas pipeline that crosses the site.

Anglia Water have been consulted and representatives of the planning and design team attended the Red Lodge Parish Council meeting on 30th July 2019 to brief Councillors.

Technical assessments relating to various aspects of the site and the proposed development have been undertaken and a number of workshops held with West Suffolk Council to inform the masterplan.

Community engagement on the draft masterplan took place between 29th April and 17th June 2022. A consultation event was held on 10 May 2022 at Red Lodge Sports Pavilion and information was also available on a dedicated website. Feedback was invited at the event, online, by email and post. Details of the consultation process and the responses received are contained in the separate Statement of Community Involvement prepared by Engagement Matters.

All comments received in response to the consultation have been considered and the masterplan amended, as appropriate. The proposed changes are set out in the Statement of Community Consultation and are included in this final version of the masterplan.

FRAMEWORK PLAN

THIS SECTION ILLUSTRATES THE DESIGN PRINCIPLES AND PARAMETERS OF THE MASTERPLAN AND ADDRESSES THE REQUIREMENTS SET OUT BY WEST SUFFOLK COUNCIL.

Masterplan Design Principles:

The first section explains the design principles and concepts that sets a framework masterplan identifying distinctive character areas and explains:

- land use principles;
- landscape principles;
- access and movement;
- urban design principles for each key space and character area;
- sustainable design principles
- general car parking principles;
- general residential block principles.

Technical Details:

The second section provides Masterplan Parameters as listed below and further information about the employment, access and movement and blue infrastructure. The parameters include:

- land use and infrastructure;
- residential density; and
- building heights;

MASTERPLAN DESIGN PRINCIPLES:

Figure 7 shows the Framework Plan for the site. It takes account of the feedback received from consultations and stakeholders, technical assessments and design principles. It shows an indicative framework for developing the site as required by the SALP.

Underpinning the vision for the site, there are a series of key concept diagrams illustrating the design rationale behind the Framework Plan. The concepts have evolved responding to the site constraints and context. These concepts are illustrated and explained in following pages.

Noise Mitigation Strategy

1

2 Proposed acoustic fence

3

Existing bund

- Landscape acoustic barriers to mitigate the noise from A11

FIGURE 7 FRAMEWORK PLAN



MASTERPLAN DESIGN PRINCIPLES:

Land Use Principles:

SALP Policy SA10 requires the provision of 300 dwellings, 8ha of employment land, 3ha for a new primary school and other appropriate uses.

The development accommodates various uses. The consideration of the most sustainable and feasible location for each use should be the starting point for structuring the masterplan.

- The most suitable location for employment is the land between the existing business park and A11. It is easily seen, accessible and a use that is less sensitive to road traffic noise.
- The most suitable location for the local centre is near to the existing school drop off / pick up point. Its location is easily accessible by the existing community as well as the new development.
- Creation of a green corridor which not only connects to the wider landscape structure, but also enables the retention and integration of the existing trees, gas pipe line and the proposed attenuation;
- Creation of open space that provides a range of leisure activities and amenity space for both new and existing residents, including potential for children's play.

Landscape Principles:

- Create two linear parks with different characters to the north and east of the development that are linked and designed to be attractive to daily dog walking.
- Create linear green links between proposed open spaces and with the wider landscape structure.
- Green street with additional width to accommodate 8m verge for avenue tree planting.
- Multifunctional spaces providing inclusive activity for all age groups including LEAP.
- Road noise from the A11 requires acoustic attenuation. Existing bund adjacent to the A11 to be retained and extended with acoustic screen and landscape buffer created using indigenous planting. In addition, naturalised mounding within the northern park will further attenuate traffic noise, as well as filter views of the employment area from the residential development and add visual interest.

FIGURE 8 LAND USE CONCEPT PLAN



FIGURE 9 LANDSCAPE PRINCIPLES CONCEPT PLAN



Access and Movement

- The primary vehicular access will be from Warren Road, with a separate access to the local centre.
- Provision of a footpath/cycle route well integrated with the strategic foot/cycleway network and the proposed landscape strategy. They should link to the proposed local services, facilities, employment and open spaces.
- Cycle and footpath connections should be made safely avoiding crossing the roundabout.
- Emergency access/pedestrian/cycle links to be provided to SALP site SA9(b) and the existing vehicular access to the commercial premises east of SA9(b) is to be retained.

Urban Design Principles

- Gateway: Creation of a gateway through the position of built form and public realm to emphasise arrival at the site.
- Legibility: Creation of landscape spaces within the site as well as creating a clear hierarchy of routes in order to increase the legibility of the development. One of the ways to achieve this is to define key frontages which are particularly prominent to the appearance. Key building frontages would contribute towards creating a unique and memorable experience of distinctive quality and character. Particular attention should be paid to the building massing and architectural style at key vistas.
- Key Spaces and frontages: Key frontages should be used to address key movement routes and to define character areas. Through the variation in building line, small informal spaces should be created within the streetscape.
- Urban Form: A clear block structure responding to the existing site features has been adopted to increase efficiency. Creation of perimeter block structures defining main movement corridors and integration of proposed development and existing landscape features.

FIGURE 10 ACCESS AND MOVEMENT CONCEPT PLAN

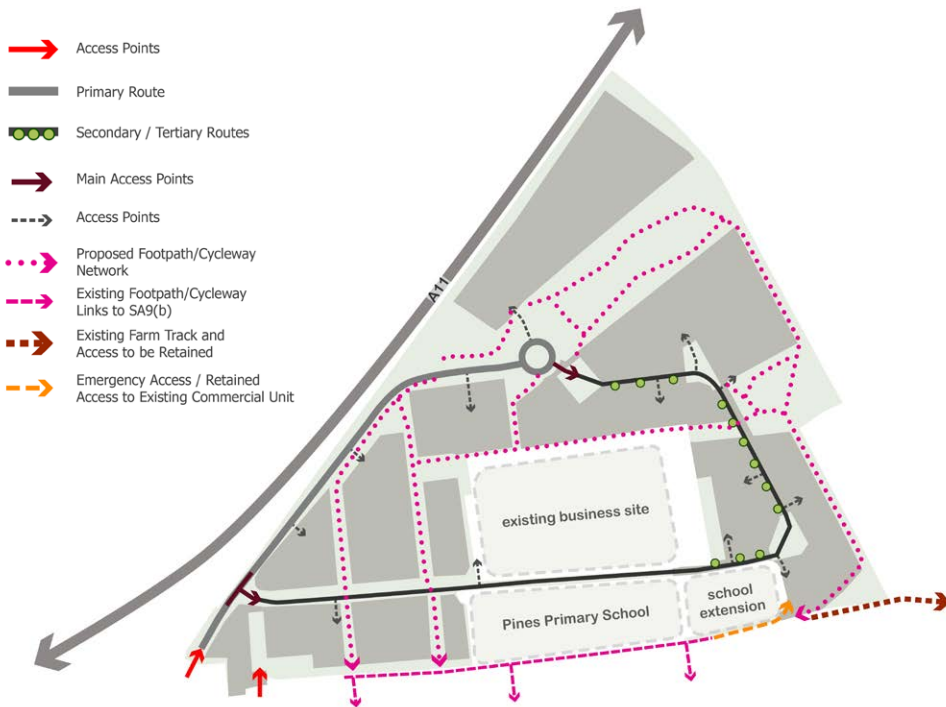


FIGURE 11 URBAN DESIGN PRINCIPLES CONCEPT PLAN



Sustainable Design Principles

Context

The NPPF makes it clear that the purpose of the planning system is to contribute to the achievement of sustainable development. The policies within the NPPF, when taken as a whole, constitute the Government's view of what sustainable development in England means in practice for the planning system.

The NPPF identifies that there are three objectives to sustainable development: economic, social and environmental and that they are independent and need to be pursued by the planning system in mutually supportive ways.

Paragraph 126 states that good design is a key aspect of sustainable development, creates better places to live and work and helps make development acceptable to communities.

Forest Heath Core Strategy Policy CS4 encourages all new developments to deliver high levels of building sustainability, whilst Policy DM7 of the Joint Development Management Policies Document requires all new development proposals to adhere to broad principles of sustainable design and construction and optimise energy efficiency.

The Suffolk Climate Action Plan was first published in July 2012 and sets out the challenges and potential impacts for Suffolk, as well as a series of actions to mitigate and adapt to climate change. The 3rd Plan was published in March 2017. As an extension to this Plan, the Suffolk Climate Emergency Action Plan has most recently been published and is working towards carbon neutrality by 2030. It follows the declaration of an Environment and Biodiversity Emergency in West Suffolk in 2019.

Building regulations are set to change from 2025 with the introduction of the Future Homes Standards requiring significant reductions in carbon emissions.

Sustainable Design Principles

The climate is changing and the design of this development must recognise this. Consideration will be given to a range of sustainable design measures, including but not limited to:

- i. Energy efficiencies;
- ii. Sustainable materials;
- iii. Water conservation and efficiencies;
- iv. Waste minimisation;
- v. Promotion of healthy communities;
- vi. Promotion of sustainable transport.

Energy Efficiency

To deliver reductions in carbon emissions and energy efficient new homes and buildings consideration will be given to:

- A fabric first approach to design and construction
- The use of renewable and low carbon technologies
- Optimisation of dwelling orientation aided by both good passive solar and thermal design
- The use of highly rated home appliances (where fitted) and low energy lighting

Utilisation of Energy Efficient Building Materials

The production, use and disposal of building materials accounts for significant quantities of energy and resources and consideration will be given to the use of sustainable materials.

The Green Guide provides a simple tool as an aid in considering the environmental implications of building materials. It applies a weighting system to calculate a summary score on a scale from A+ to E, with A+ rated specifications having the lowest overall environmental impact.

All building materials to be used will achieve a Green Guide rating of A-C.

Water Efficiency

New homes and buildings will include water efficient fixtures and fittings to reduce water use and achieve the water consumption rate of 110 litres per person per day in line with the Council's Local Plan policy requirements.

Minimising Waste

Consideration will be given to minimising site construction waste, separating waste for re-use and recycling.

A balanced cut and fill scheme will be designed to implement the undulating grass mound landscaping of the Pines Park, minimising the amount of material that will be either imported or exported from the site.

Once the development is complete the provision of recycling bins will encourage residents to further reduce household waste.

Promoting Healthy Communities

A mixed-use local centre, accessible green spaces, employment opportunities and a range of house types, in addition to the existing primary school, will provide the infrastructure that helps to create a vibrant integrated new community. The proximity of the open spaces will enable residents and visitors, to enjoy a range of leisure activities, and footpath connectivity, which can contribute to a healthier lifestyle and better well-being.

Promoting Sustainable Transport

The masterplan encourages walking, cycling, play and sports through the provision of pedestrian and cycle routes that link to local services and areas of open space.

Electric vehicle charging infrastructure will be provided in new homes and for employment uses to facilitate the use of low carbon transport.

The development is located within an accessible distance of the existing bus service in Red Lodge and will be designed to accommodate a future bus route.

Energy Statement

An Energy Statement will be submitted with the planning application and set out details of the energy strategy. It will be prepared in accordance with the principles of the energy hierarchy, prevailing planning policy and building regulation requirements.

KEY SPACES AND CHARACTER AREAS

1- FORMAL CHARACTER

Built Form

- Typologies: Terraced and semi-detached forms would be the most prevalent typologies along the Spine Road with occasional opportunities for apartments marking the corners or key spaces as part of creating variation within the street scene;
- Heights: Building heights should be consistent and typically comprise 2 and 3 storey units as part of creating a legible spine through the heart of the site. 3 storey buildings would be particularly appropriate as part of a defining of the key spaces, vistas and the gateway;
- Rhythm and Orientation: The Spine Road should have a strong regular rhythm and form, with ridgelines typically running from front to back to allow roof pitches that are capable of taking advantage of the southerly aspect;
- Parking: A combination of on plot parking, within 'drive throughs' and within courts would address the allocated parking requirement on the Spine Road, integrated with the spacing of avenue trees to maintain the consistency of this feature. Visitor parking could be provided within verges, integrated with the avenue planting or on side street.
- Building Line: A regular building line and consistent set-back would reinforce the rhythm within the street;
- Boundary Treatments: Boundary treatments could comprise a combination of walls and railings with planting behind;

Public Realm and Landscape

- Green verges on each side of the street one of which could incorporate avenue tree planting where appropriate;
- A cycle way should be incorporated on one side of the street to provide alternative ways of movement and connect into the existing network beyond the site;

Materials

- Roof: Roofing materials to the Spine Road should be of a consistent type and material to create a unified appearance and help differentiate this key frontage from others within the development. The roofing material should draw upon influences from the local vernacular and could comprise plain tile or slate/ reconstituted slate tiles;
- Walls: Walling materials should be limited to a simple palette of materials comprising brick and render as the dominant elements.

- A consistent set-back would ensure the buildings interact positively with the street and are consistent with the high density, more urban character of the Spine Road.
- Buildings should turn the corners by providing an active frontage to both streets with the entrance to the building on the Spine Road.
- Buildings on both sides of the street should be designed holistically so that they read as one.
- Corners would be an appropriate location for a landmark building, particularly where associated with a key space.
- Private amenity space should be provided to the rear of the dwelling as part of providing a clear distinction between public fronts and private backs.
- Key spaces could punctuate the Spine Road, providing focal points that would aid wayfinding and creating a sense of place



FIGURE 12 FORMAL FRONTAGE CHARACTER AREA LOCATION PLAN



2- PARK EDGE FRONTAGE - Semi Formal Character

Built Form

Typologies:

A combination of short terraces and semi-detached units with integral garages would provide a strong form for this edge;

Heights:

Building heights should be consistent of typically 2 storey units but the roof line could change within the building for example above the garages.

Rhythm and Orientation:

Consistency of typologies within groupings together with a consistent approach to parking will generate a strong rhythm to the edge. Orientation is consistent and ridge lines should be designed to take advantage of the south facing aspect.

Building Line:

A consistent building line and set-back within groupings would reinforce the rhythm of this edge, the building line could be recessed where there are integral garages;

Boundary Treatment:

Low hedges and planting could provide a defensible edge and contribute to the soft landscape feel;

Parking:

Parking on plot and in integral garages.

Public Realm and Landscape:

Low key shared surface drives could provide access to the dwellings;

Irregular tree planting on the edge of the open space could filter views of the development edge.

Materials:

Roof: Roofing materials could be consistent within groupings and respond to precedents from the local vernacular;

Walls: A small selection of walling materials could create an overall unified appearance.

- Buildings should positively address the street and open space beyond, providing a clear public front and secure private back;
- Allocated parking should be provided on plot;
- Rooms above the garages creates variety in roof line;
- Strong rhythm but informal elevational treatment;
- Consider asymmetric frontage to emphasise the informality;
- Shared surface streets would provide a transition to the open space beyond and provide direct access to individual dwellings .



FIGURE 13 ELEVATIONAL DESIGN PRINCIPLES DIAGRAM

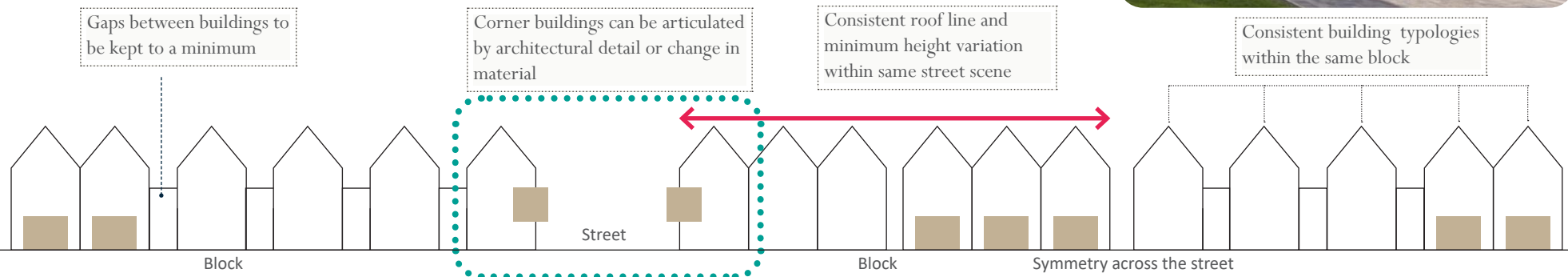


FIGURE 14 PARK EDGE CHARACTER AREA LOCATION PLAN



3- GREEN EDGE FRONTAGE - Informal Character

Built Form

Typologies:

A mix of semi-detached and detached units would be an appropriate feature to this edge.

Heights:

Building heights would be typically 2 storey and create an informal outer edge character with a continuous street enclosure that shows some variation in set backs.

Rhythm and Orientation:

Variation of typologies and orientations within groupings would create a diverse and interesting street scene.

Building Line:

The Green edge should have an irregular building line with variations in set backs.

Boundary Treatment:

Soft landscape planting would provide a defensible but soft edge to the open space opposite.

Parking:

Parking should be mainly on plot with visitors parking provided on street allowing the car to be accommodated as an integral part of the street scene.

Public Realm and Landscape:

Low key shared surface drives provide access to the units, integrate traffic calming into the street design with changes in alignment and surface materials. Changing set-backs would give this edge a soft feel. Tree planting would be provided in informal groups.



Materials:

Roof: A combination of materials found within the local vernacular including plain tile, slate, recon slate and pantile would reinforce the informality of the edge;

Walls: A variety of walling materials would typify the green edge character, texture and colour drawing upon the local vernacular.



- Buildings should positively address the street and open space beyond, providing a clear public front and secure private back.
- Informal character
- Building form mainly detached and semi-detached
- Variety in set-back
- Variety in roof line
- Mainly 2 storeys but with variety in building height
- Soft boundary treatment



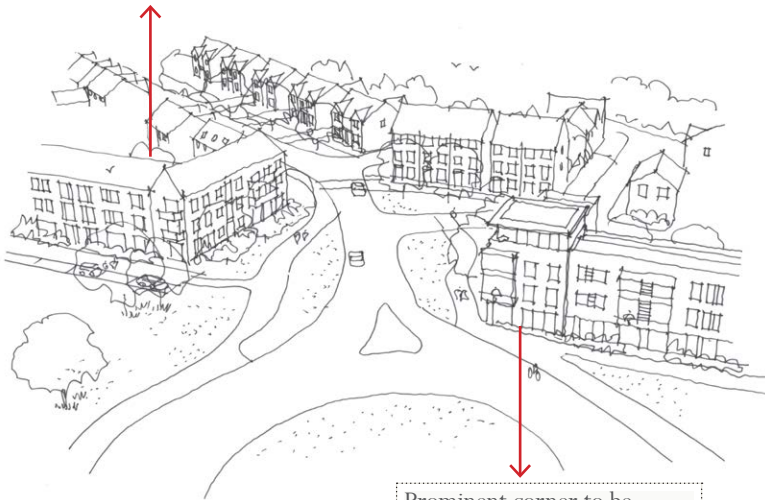
FIGURE 15 GREEN EDGE CHARACTER AREA LOCATION PLAN



KEY SPACES 1 - THE GATE WAY

The gateway to the development is located on the northern end of the Formal Character. Building layout and landscape should therefore respond positively to the access point created.

Landmark buildings to emphasise site entrance



Prominent corner to be articulated by a key landmark building

- Buildings should positively address the street and open space beyond, providing a clear public front and secure private back;
- Formal character;
- Building form: flats and terraces;
- Consistent set-back;
- Consistency in roof line around the square;
- Appropriate building height to define the entrance space created;
- Clear boundary treatment.

KEY SPACE 2 - GREEN STREET

The Green Street is one of the side streets forming the visual and pedestrian connection to the open space located north of the development. It functions as an important strategic linkage, connecting two main green spaces and should therefore have a wide verge with large trees on one side. The verge should not form part of the drainage strategy.

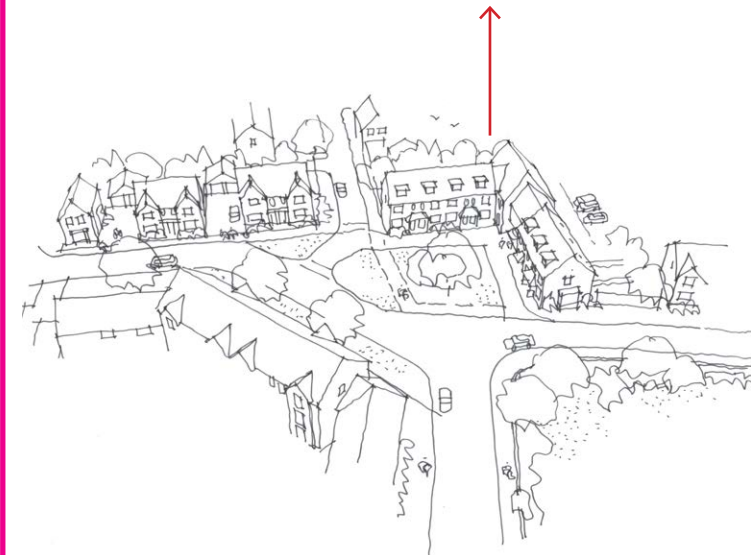
- Buildings should positively address the street;
- Semi formal character;
- Building form mainly detached and semi-detached;
- Consistent set set-back - framing the green verge;
- Variety in roof line;
- Mainly 2 storeys but with variety in building height;
- Soft boundary treatment



KEY SPACE 3 - NEIGHBOURHOOD SQUARE

The Neighbourhood Square should be of formal character area on one side to reflect the Spine Road and informal character on the other. Buildings should be mainly 2 storey in height with an increase to 3 storey at the corners, detached and semi-detached with flats and have variations in roof line. There should be a change in surface material and soft boundary treatment to reflect this key space.

Informal frontage with a high degree of enclosure



- Informal character on one side would indicate change of character;
- Building form should turn the corners - mainly detached and semi-detached with flats;
- Change of surface;
- Variety in roof line;
- Mainly 2 storeys but with increase in building height at corners;
- Soft boundary treatment.

FIGURE 16 LOCATION OF KEY SPACES



4-LOCAL CENTRE - HUB

The North Red Lodge Local Centre should be a focal point for the new development and could provide a mix of local services, health and community facilities.

The new development should provide a Local Centre with a strong sense of place focused around a new shared surface square. The Local Centre buildings should be configured to define and provide a positive relationship to this space consistent with creating an attractive and vibrant heart to the new community at Red Lodge. The new hub should make appropriate provision for car parking and the school drop off.

The space in front of the buildings should be capable of accommodating multiple activities at special occasions such as local markets, etc. as well as serving the development as a school drop off and pick up area and car park. There are opportunities to create sitting areas provided on shared surface space, spill out space in front of the shops, display areas for public art and a small play area.

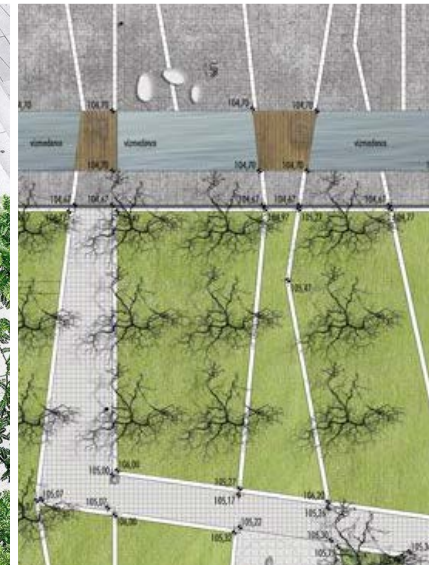
The size, scale and form of the Local Centre will be subject to land availability and capacity at detailed design stage and market interest.

Design Inspiration Images



DESIGN PART OF THE CAR PARK IN UNITY WITH THE REST OF THE SQUARE OFFERING A MULTI FUNCTIONAL SPACE FOR THE COMMUNITY

Design Inspiration Images



GENERAL CAR PARKING PRINCIPLES

All parking will form a well-integrated element of the design so that it does not undermine the character of development being created. Three categories are considered:

- Residential Parking
- Non-Residential Parking
- Cycle Parking

Overall, the street frontage, boundary treatments, on-plot landscaping, access points, setbacks and ancillary buildings / garages should be positioned and designed to ensure that vehicles and their driveways are not a prominent component of the street.

Car Parking

The general approach is to ensure that the proposed mix of on-plot and off-plot parking solutions are integrated within the urban form, architecture and public realm, with minimal visual intrusion and obstruction to pedestrians. Car parking will be designed with reference to best practice guidance published by English Partnerships in 'Car Parking What Works Where' and Suffolk County Council's Parking Guidance. It will be safe and secure, with natural surveillance provided by overlooking properties. Spaces for cars should not interrupt service or emergency vehicle access or create unsafe conditions for pedestrians.

Within the residential areas a mix of on street, off street and on plot parking spaces will be provided. Some on street spaces will be available for visitor use. Off street spaces will normally be provided in small groups serving five to eight homes. There will be no limit for apartments or mixed use blocks, but care will be taken with the design to ensure security is not compromised. On plot spaces will be provided for larger dwellings and including garages, integral garages and spaces.

Parking provision will be in accordance with adopted standards and will include sufficient electric charging infrastructure.

Bike Storage

In line with Secure by Design 2019, and the Suffolk Design Guide, each dwelling should have secure cycle storage. When garages are provided, the cycle storage should be integrated within the garage. Where there is no garage, the following storage options will be considered:

- Cycle parking within the house or apartment block. Where this solution is proposed the scheme drawings must show how appropriate space, which is specifically designed for cycle storage, will be provided within the building(s); within the rear garden areas or courtyards in a secured and protected location from the weather.
- Cycle parking provision will be provided in accordance with Council's cycle parking standards.

Secure, accessible, weatherproof and aesthetically designed bike stores will be provided in accordance with adopted standards for all other uses, such as the neighbourhood centre facilities and within the employment areas.



Bike racks to be integrated to the public realm



Space for bikes within garages or as part of the dwelling fabric



Car dominant public realm without soft landscape



In line parking adjacent to verge and tree planting

GENERAL RESIDENTIAL BLOCK PRINCIPLES

A number of block types could be provided within the development. Back to back blocks are the most common typology. Blocks located along the Spine Road could be Courtyard Blocks where dwellings are generally accessed from the rear or via small parking courts. These principles should be applied to the design layouts moving forward.

Back to Back

General Character

1- The back to back block should create a clear distinction between public and private realm.

Public Realm Design

2- High quality surface materials should be used to enhance public realm and encourage pedestrian activities. Hard and soft landscaping treatment and tree planting should be used where appropriate.

Security

3- Buildings should face the public realm with front doors and/or windows to habitable rooms to give natural surveillance to streets.

4- All cars need to be overlooked from ground or upper floor windows.

Urban Form

5- Buildings should be provided around the block perimeter to define the public realm (streets, squares etc) from the private realm (rear gardens)

Active Frontage

6- Active Corners - Corner elevations should have windows, avoiding long sections of blank walls. Consideration should be given to placing the front entrance and windows to main rooms on the gable.

Parking

7- In the streets around the block there should be a mixture of visitors and on-plot spaces for private parking.

Servicing and Bike Storage

8- Provision of adequate space for refuse and recycling bins as well as accessibility to them should be considered. Generally bins should be collected from the front of properties with back to back block arrangement. Where bins are to be stored to the rear of the property, gated access will need to be provided to the rear garden from the front of property.

Each dwelling should have secure cycle storage in accordance with current standards. If there are garages then the cycle storage should be designed to be in the garage. Where there is no garage, the following storage options will be acceptable:

- Cycle parking within the house or apartment block. Where this solution is proposed the scheme drawings must show how appropriate space, which is specifically designed for cycle storage, will be provided within the building(s); or
- Within the rear garden areas.

Street Design

9- The streets should be designed to give priority to those with restricted mobility, pedestrians and cyclists. High quality materials are to be used in the streets.

10- Include breaks in lines or rows of on street parking bays typically every six to eight spaces which will include space for tree planting and make it easier for pedestrians to cross from one side of the street to the other.

Courtyard/Parking Courts

General Character

1- The internal areas will accommodate a number of uses. The amount of parking and garaging will vary according to density and must not dominate the space without regard to amenity.

2- Buildings should be provided around the block perimeter to define the public realm (streets, squares, etc.) from the private realm (rear gardens).

Landscape Design

3- Courtyards and parking courts should be secure places with suitable hard and soft landscape treatments which include tree planting where space allows. Internal courtyards present opportunities to create semi-private multi-use spaces.

Parking

4- A mix of on-street and on-plot spaces. Care should be taken in the design of courtyards/parking courts to ensure that security is not compromised.

5- Parking Bays - Include breaks in lines or rows of on street parking bays typically every six spaces which will include space for tree planting and to make it easier for pedestrians to cross from one side of the street to the other.

6- All cars should be visible from ground or upper floor windows.

Security and Active Frontage

7- The design of courtyards in residential areas should discourage access by people other than adjoining occupiers and visitors. Any entrance ways should make it clear to non-residents that the courtyard is private (for example a narrow gap through the buildings with a first

floor development above, and or gated access). Opportunities for casual surveillance of courtyards should be maximised.

8- Buildings should face on to the public realm with front doors and windows to main rooms giving natural surveillance and activity to the public realm.

9- Active Corners - Generally, corner elevations should have windows, avoiding long sections of blank walls. Consideration should be given to placing the front entrance and windows to main rooms on the gable.

Servicing and Bike storage

10- Provision of adequate space for refuse and recycling bins as well as accessibility to them should be considered. Generally bins should be collected from the front of properties with back to back block arrangement. Where bins are to be stored to the rear of the property, gated access will need to be provided to rear gardens from the front of the property.

11- Each dwelling should have secure cycle storage in accordance with current standards. If there are garages then the cycle storage should be designed to be in the garage. Where there is no garage, the following storage options will be acceptable:

- Cycle parking within the house or apartment block. Where this solution is proposed the scheme drawings must show how appropriate space, which is specifically designed for cycle storage, will be provided within the building(s); or
- Within the rear garden areas; or
- Within courtyards in a secured location sheltered from the weather.

Street Design

12- The streets should be designed to give priority to the disabled, pedestrians and cyclists with the use of high quality materials.

13- The street width and length should be varied according to the prominence of the route and housing density. This will allow a wider range of parking types to be used and will help to vary street character.

Mews Courtyards and Mews Streets

General Character

1- Mews Block would consist of a continuous frontage with a mews street to the rear with parking spaces and garages on one side and a back to back block on the other. The mews character block would prevent the domination of spaces around the perimeter of the block by parked cars and garages.

Landscape Design

2- The selection of high quality surface materials, street trees and furniture is essential in creating a pleasant environment for residents and visitors. Particular care is to be taken in the design for the mews courtyard where dwellings are mixed with car parking.

Parking

3- In the mews there should be a mixture of parking spaces and garages for the residents.

4- All cars need to be surveilled from ground or upper windows.

5- Streets should vary in width to allow visitor parking to occur. The amount of variation will depend on street type.

6- For on-street or mews parking there should typically be no more

than six spaces without a break if perpendicular, and four spaces if parallel.

Servicing and Bike storage

7- Care should be taken to provide locations for refuse and recycling bins that are convenient for collection.

8- Each dwelling should have secure storage in accordance with current standards.

Security and Active Frontage

9- Buildings should face the public realm with front doors and/or windows to habitable rooms to give natural surveillance to streets.

Street Design

10- The streets should be designed to give priority to the disabled, pedestrians and cyclists. High quality materials are to be used in the streets.

11- Generally, corner elevations should have windows, avoiding long sections of blank walls. Consideration should be given to placing the front entrance and windows to main rooms on the gable. This applies particularly to the shorter edge of back to back blocks.

TECHNICAL DETAILS:

PARAMETERS: LAND USE AND INFRASTRUCTURE

Land Uses

The distribution of the land uses and their relationship is shown in Figure 17. The pattern of land use is derived from the evaluated opportunities and constraints and the development concepts and principles.

Residential Development

Residential use will be one of the primary land uses within the site. The net residential area amounts to 7.82h and can accommodate around 300 dwellings.

Affordable Housing and Housing Mix

Affordable housing shall be provided in accordance with adopted Local Plan policies, currently 30% under Policy CS9 of the former Forest Heath Core Strategy.

The development will need to secure the Council's recommended tenure mix of 70% rented and 30% intermediate housing and the affordable housing mix will need to meet the prevailing housing need for West Suffolk and provide a range of house types and sizes.

Employment

Employment use will be the second primary land use within the site. The gross employment area amounts to 8 ha and will accommodate a mix of Class B1, B2 and B8 uses.

Local Centre

The Land Use Plan identifies an area of 1.08ha for a Local Centre to the west of the site. The Local Centre has the potential to incorporate a range of retail and community uses including:

- retail floorspace,
- health/community facilities,
- apartments, above and next to the retail unit and,
- associated car parking and the school drop off area.

The mix of uses will form part of a detailed planning application.

Open Space, Informal Open Space and Landscape Buffers

The Master Plan shows a Local Equipped Area of Play (LEAP), located well within the green corridor and integrated with the building form. The location enables the majority of the development to be within a 400m walking distance (approximately 5min walk). Buildings will front on to the play space providing natural surveillance.

Informal open space and landscape buffers on the site, will be a mix of existing landscape features, mainly pine lines and tree belts to be retained and areas of ecological value identified for retention on the Constraints and Opportunities Plan, Figure 6 .

Education

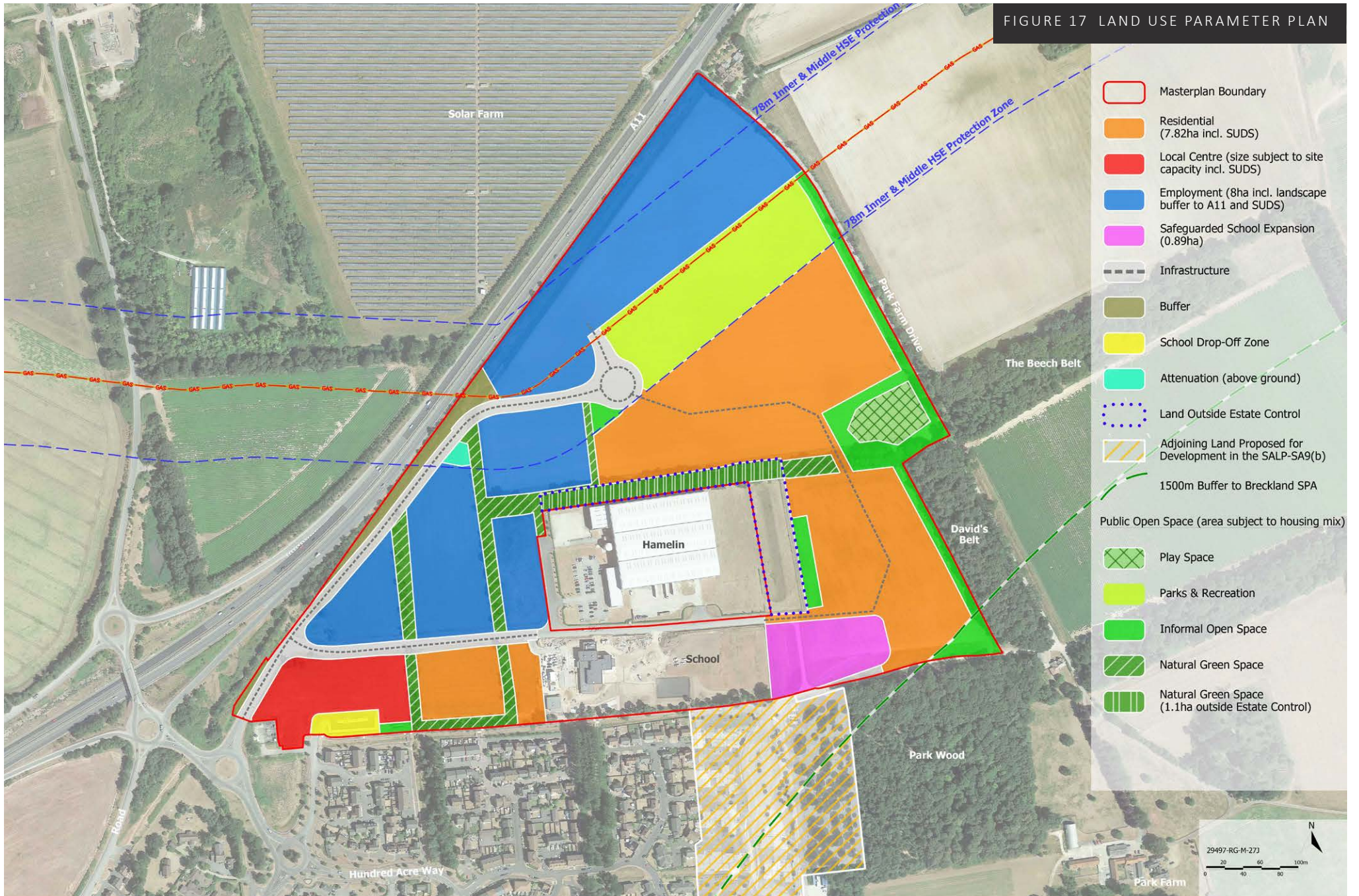
The Land Use Plan identifies an area of 0.89ha as Safeguarded for future school expansion.

Surface Water Drainage Strategy

Soakage tests have been undertaken and confirm the ground is suitable for the use of infiltration systems and soakaways.

See the Blue Infrastructure section for further details.

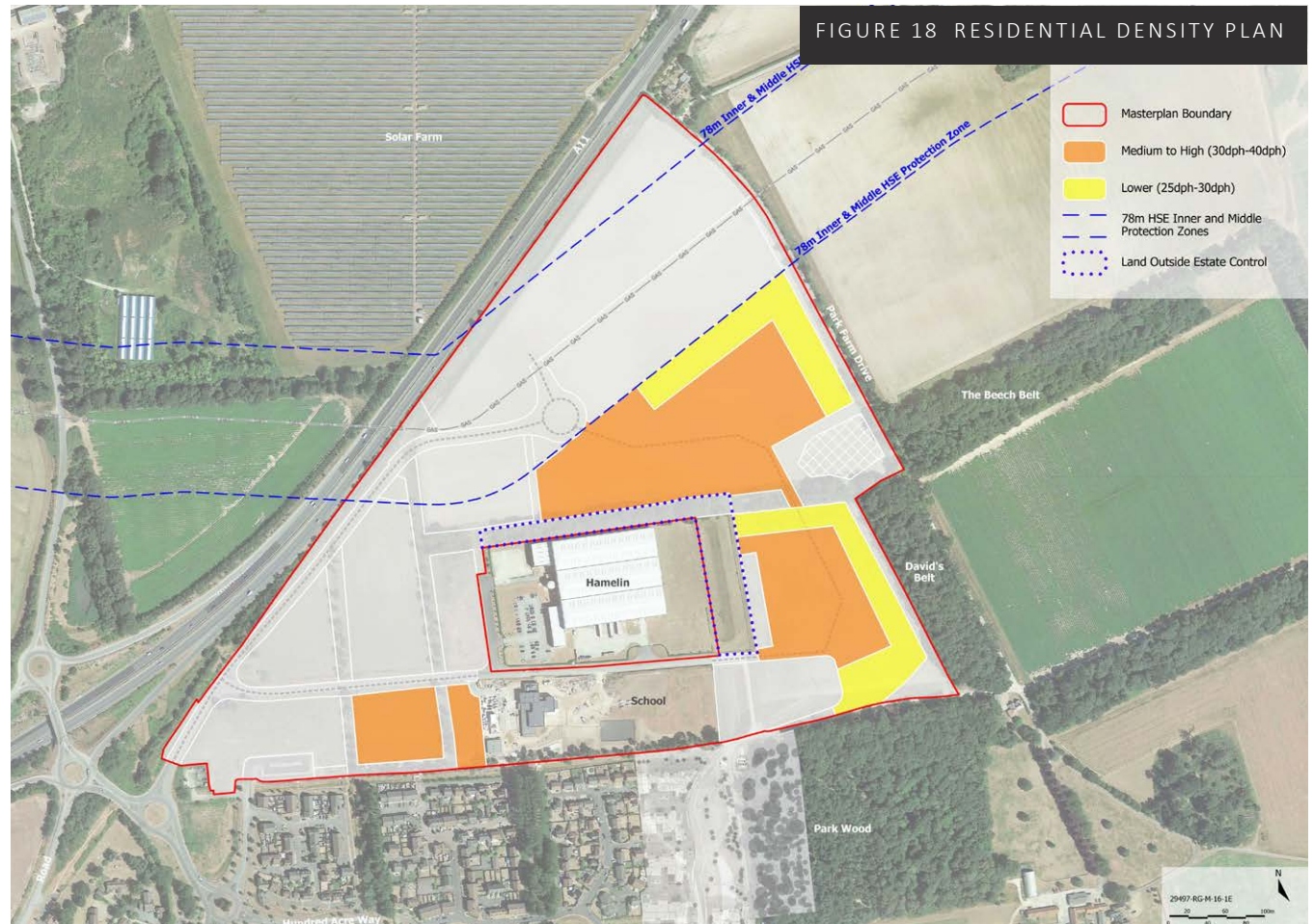
FIGURE 17 LAND USE PARAMETER PLAN



PARAMETERS: RESIDENTIAL DENSITY

Residential Density

The density of the residential development will vary depending on the character area. The residential development can accommodate around 300 dwellings and the average net density will be 35 dwellings per hectare (dph). The density parameter indicates the approximate residential density across the site which will range from 25dph to 40dph.



PARAMETERS: BUILDING HEIGHTS

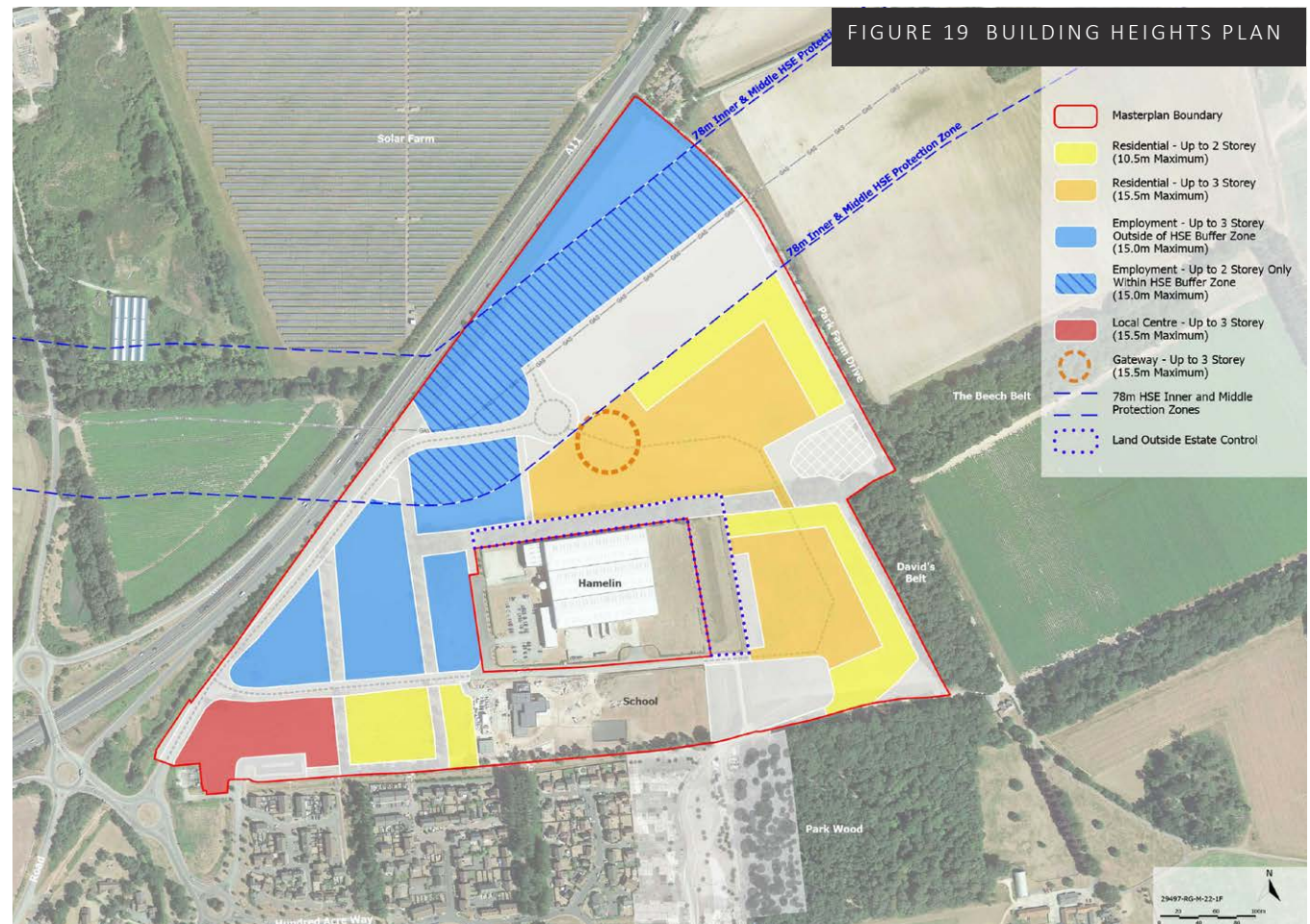
Building Heights

The Building Height Parameter Plan indicates the approximate heights of buildings across the site.

The residential building height should be mainly 2 storey with occasional 3 storey units located at key spaces and key vistas including around the gateway.

The employment building height should be no more than 2 storeys within the HSE constraints zone and no more than 3 storeys elsewhere.

The buildings within the local centre should be up to 3 storey.



EMPLOYMENT

As required by SALP Policy SA10(J) 8 hectares of employment land will be provided within the allocation site at North Red Lodge. Opportunities exist to provide any combination of the following uses:

- Class E(g): offices (i), research and development (ii), light industrial (iii) which can be carried out in a residential area without detriment to amenity (former Use Class B1 (Business));
- Class B2 (General Industrial): an industrial process other than one falling within Class E(g)(former Class B1);
- Class B8 (Storage and Distribution): use for storage or as a distribution centre.

The employment site is located on the A11, within the Norwich to Cambridge Tech Corridor, as identified in the West Suffolk Employment Land Review 2021 (as amended). This Review identifies its potential for B1c (light industrial)/B2/B8 uses. This accords with the market interest that the Landowner has received.

The employment land will be located to the west of the allocated site, 'north' and 'south' of a new internal access road as shown on Figure 20.

The western boundary of the northern employment area, along the A11, is partly screened by an existing bund which will be retained and planted with native trees, shrubs and grasses. The southern boundary is currently open and the employment area would front the northern park.

Views of the southern employment area from the west will be filtered by existing trees and two pine lines bisect this area compartmentalising it. Its southern and north eastern boundary, where it will front new residential development is open. Naturalistic mounding within the

northern park will help to attenuate noise from the employment area and filter views from the proposed residential development.

Employment Mix and Delivery

Having regard to site constraints and the proximity of more sensitive residential uses, Class B2 and B8 employment uses will be located in the northern employment area only. Light industrial uses (Class E(g) (iii)) will be located within the southern employment area. It is not anticipated that Red Lodge North will be a location for offices other than those that are ancillary to another employment use.

The northern and southern employment areas as shown are broadly the same size and as such will each accommodate 50% of the total new employment as follows:

- Northern employment area: 50% Class B8 and /or B2;
- Southern employment area: 50% Class E(g)(iii), to include the provision of some small business units the details of which would form part of a future planning application.

Delivery of the Red Lodge North development as a whole will be phased as shown on the Phasing Plan (Figure 47). The majority of the southern employment area should be delivered as part of Phase 1, with a small area to the north and the northern employment area delivered as part of Phase 3. This phasing does not prohibit the early delivery of the northern employment area in response to market interest.

Design Inspiration Images



FIGURE 20 EMPLOYMENT



ACCESS AND MOVEMENT

Inclusive Design

The development should comply with 'Planning and Access for Disabled People: A Good Practice Guide' (ODPM 2003). The aim of inclusive design is to remove barriers which create undue effort, separation or special treatment, and enable everyone to participate equally in mainstream activities independently with choice and dignity. Although the principles of inclusive design have emerged from the disability movement, inclusive design is relevant to all members of society, such as older people, children and carers of young children.

From the outset the design of the development should include the fundamental principles of inclusive design, including the following:

- Provide level access to all buildings;
- Create safe pedestrian crossings;
- Ensure changes in level including ramps have a maximum gradient of 1 in 20 (5%); and
- Provide good pedestrian links to existing bus stops

Access and Movement

The access and movement strategy for the site will be developed in line with guidance given by Suffolk County Highways and wider planning and design guidance such as that set out in Manual for Streets.

The access and movement strategy will be underpinned by a set of key movement principles which are:

- to control the flow, speed and circulation of traffic to create a safe environment where pedestrians and cyclists have priority;
- to promote sustainable travel by providing a network of permeable, safe, convenient and attractive pedestrian and cycle routes that offer real alternatives to the car for local journeys;
- to accommodate car movements but through design, reduce the dominance of the car in the built environment; and
- to minimise the dominance and influence of car parking on the urban form.

The access and transport strategy will seek to provide a balanced and integrated solution that offers opportunities for movement by alternative modes than the car and thereby provide the foundations to support sustainable and accessible development opportunities.

The access and movement parameter plan indicates the location of the primary vehicular routes, pedestrian and cycle routes and site access points.

Site Access

The main vehicular access, including HGV access to the employment site, will be from the middle roundabout on Warren Road to the south west.

Vehicular access to the local centre will be from the southern roundabout at the junction of Warren Road and Hundred Acre Way, past Elderberry Road.

These accesses will be linked by an internal loop road which will be designed to accommodate a bus route.

There will be no change to the current access arrangements to The Pines Primary School and a separate school drop-off facility will continue to be provided.

The roundabout junction shown on the Figures within the masterplan is illustrative and there may be other appropriate design solutions. The design of this junction shall have regard to the site constraints, highway safety, pedestrian and cycle movements and the design principles for the gateway to the northern residential area.

Emergency Access

Emergency access should be provided to the south east to SALP Site SA9(b) known as Land East of Red Lodge: North.

Vehicular access to the commercial unit to the east of SA9(b) must be retained.

Farm Access

The existing farm access to Park Farm must be retained and integrated with the development and details provided as part of any planning application. An example of how this could be achieved is shown in Figure 21.

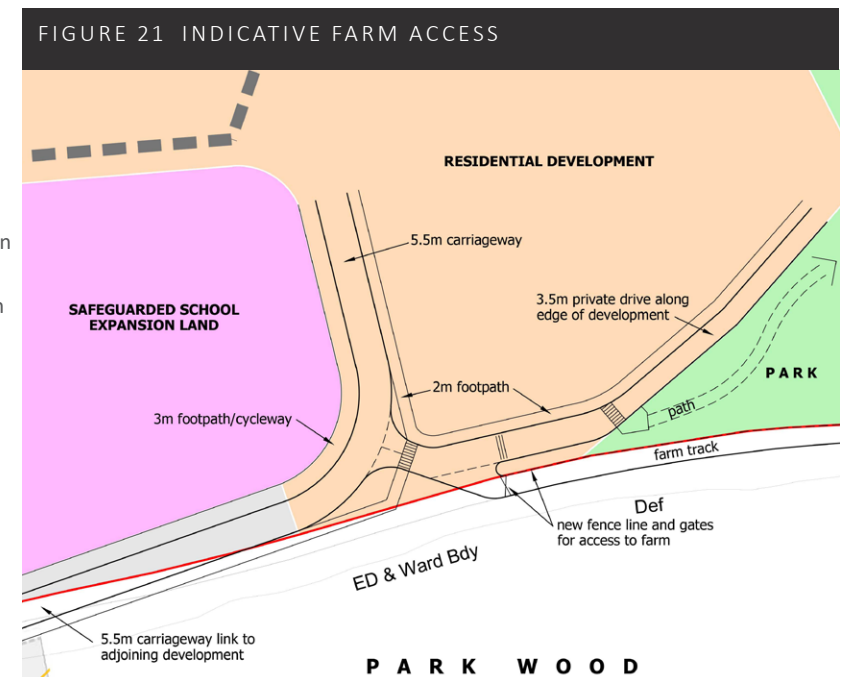


FIGURE 22 STREET HIERARCHY, ACCESS AND MOVEMENT PLAN



Public Transport

The development site is well positioned to encourage journey's by bus. Figure 24 illustrates the development in relation to existing public transport services and shows that the majority of the site is within an 800m radius (10min walking distance) of the nearest bus stop.

The number 16 bus service is routed along Hundred Acre Way to Bury St Edmunds via Mildenhall to the north and east and Newmarket to the south.

The proposed main route through the site should be designed to accommodate a bus route. Bus stop provision and positions should be reviewed as part of the detailed design to ensure they are located within appropriate walking distances for users of the site.

Access for Pedestrians and Cyclists

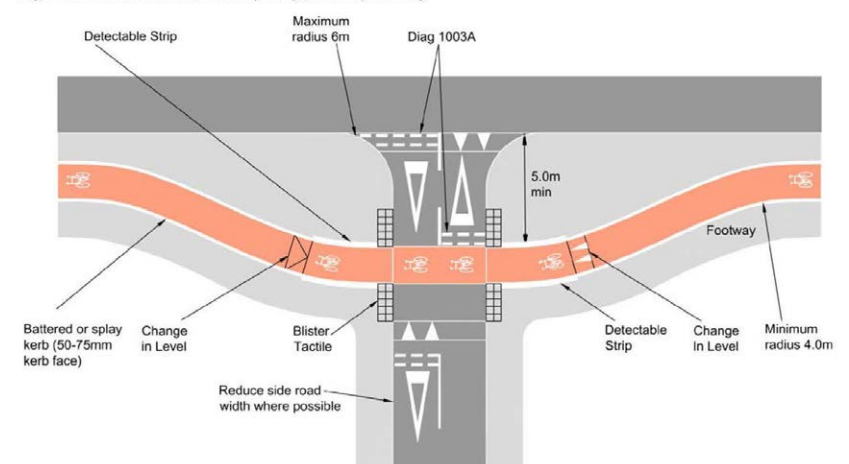
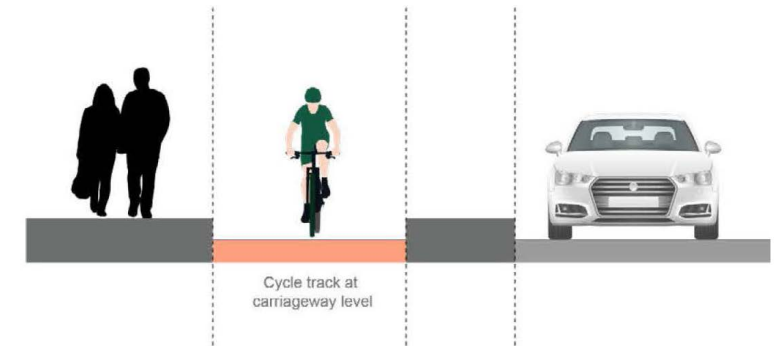
The development will encourage walking and cycling through a series of connected routes. A segregated cycle and footpath network will provide direct and safe links to the new local centre, employment areas, open space and The Pines School. They will also connect into the existing networks in Red Lodge and link to St Christopher's Primary School, the existing village centre and sports facilities.

Figure 22 identifies the links to the existing network of pedestrian and cycle routes adjoining the site and illustrates how they can be extended in to and around the site. The details will form part of a planning application.

Design Inspiration Images



FIGURE 23 EXAMPLE OF CYCLE PRIORITY JUNCTION DESIGN



Cycle Routes

Local Transport Note 1/20 on Cycle Infrastructure Design sets a clear ambition to make cycling and walking the natural choices for short journeys or as part of a longer journey with supporting objectives to increase cycling and walking levels.

Based on this guidance, all cycle routes should be designed so the cyclist has priority over junctions as illustrated in Figure 23.

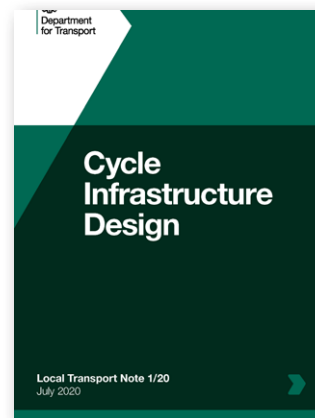
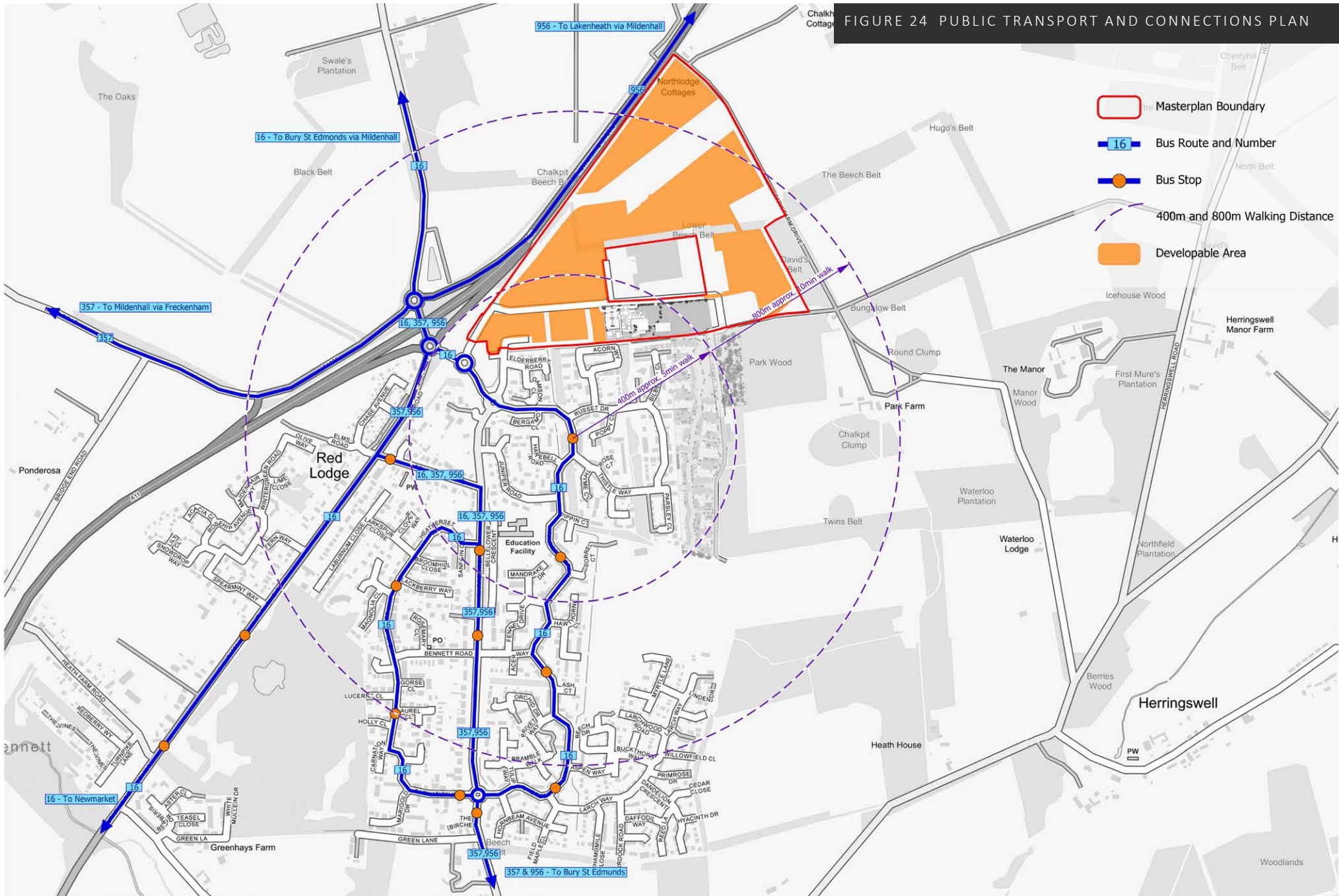


FIGURE 24 PUBLIC TRANSPORT AND CONNECTIONS PLAN



Street Hierarchy

The street hierarchy of the new development should aim to create a legible movement network with a series of main routes, side streets, mews and private drives as shown on the following pages.

Access Road

Access Road is the largest street within the development providing the link between the new developments and the wider road network. There is access to primary and secondary streets and to Employment Streets. Design to cater for public transport services.

Details must be designed in conjunction with Suffolk Design Street Guide.

Design Example Image



FIGURE 25 INDICATIVE LOCATION AND JUNCTION DESIGN

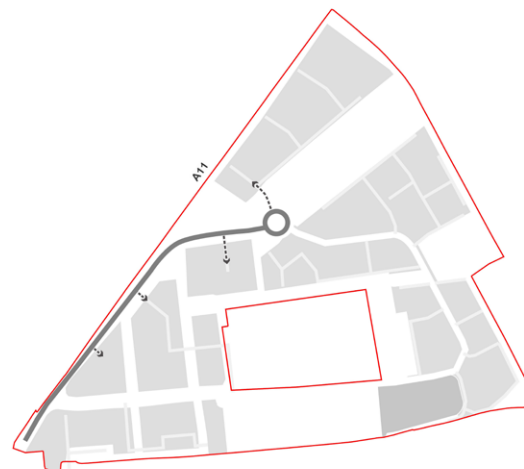


FIGURE 26 INDICATIVE ACCESS ROAD SECTION



- No single access to individual employment blocks
- Should discourage on carriageway parking
- Carriageway should be min 6.5m in width
- Trees are within the open space or verge

Employment Street

Employment Street would primarily be used as main access routes for visitors, staff and lorries to each employment unit.

Employment units should have various set-backs from the street, ranging from 0 - 8 metres to accommodate trees and low level planting and to provide a buffer zone between the street and employment area.

Surface of Employment Streets will typically be Tarmac.

Details must be designed in conjunction with Suffolk Design Street Guide.

Design Example Image



Primary Carriageway (Existing and Proposed)

The Primary Carriageway forms the south end of the spine road that provides the key vehicular, pedestrian and cycle route through the site. Where there is residential fronting to this street the frontage will have a consistent formal character. It will be designed to cater for public transport services. There will be footpath on one side, and a shared surface segregated footway/cycle way on the other side. Both will be separated from the carriageway by a grass verge (see figure 28 section A). When passing between Hamelin and The Pines Primary School, as the space is limited, there will be no verge (see figure 29 - section B).

Direct access for vehicles to individual properties fronting onto the street will be limited. Primary Street must be designed in conjunction with Suffolk Design Street Guide.

FIGURE 28 ILLUSTRATIVE PRIMARY CARRIAGEWAY STREET SECTION (A)

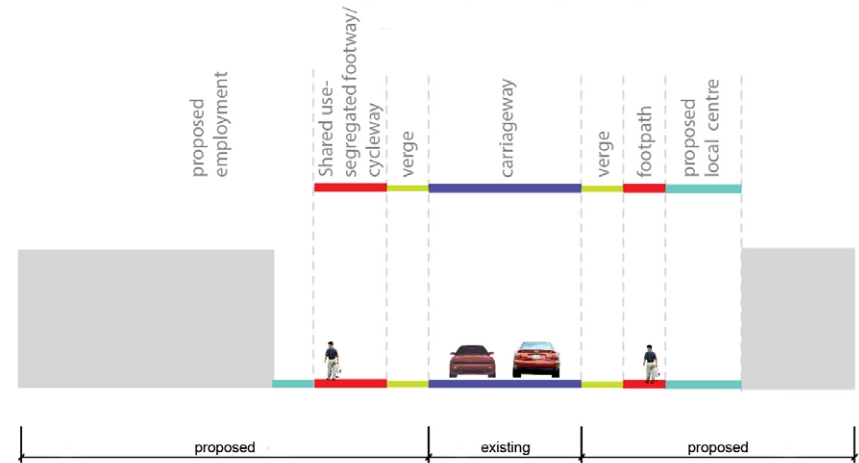
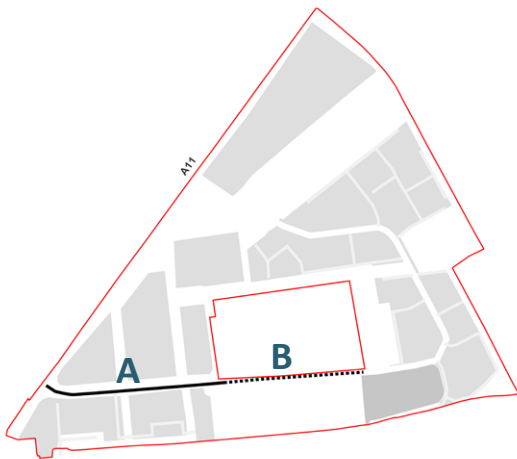


FIGURE 27 INDICATIVE LOCATION



Must provide separation for cyclists if the carriageway is alongside a cycle route

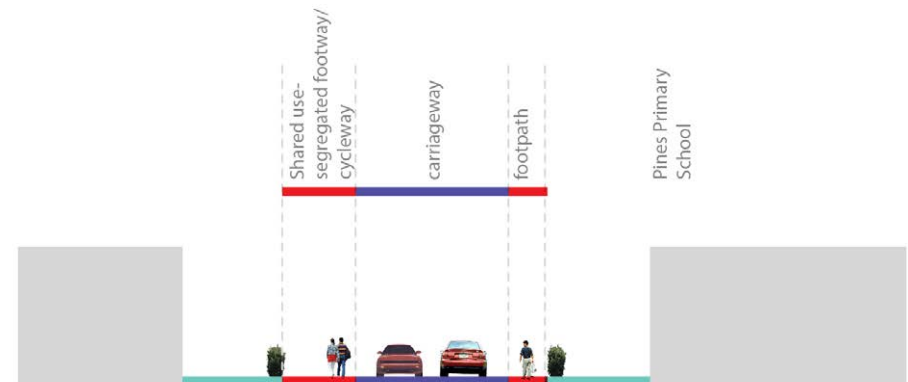
Trees are within the open space

Carriageway should be min 6.5m in width

No single access to individual employment blocks

Should discourage on carriageway parking

FIGURE 29 ILLUSTRATIVE PRIMARY CARRIAGEWAY STREET SECTION (B)



Design Example Image



• Existing Road



Secondary Carriageway

Secondary carriageway acts as a transition between the tertiary road and the primary road. Whilst the characteristics are the same, the carriageway gently narrows down providing traffic calming entering the residential parcels of the development. The street provides access to individual plots. Cyclists are directed away from the carriageway. Details should be designed in conjunction with Suffolk Design Street Guide.

FIGURE 30 INDICATIVE LOCATION PLAN

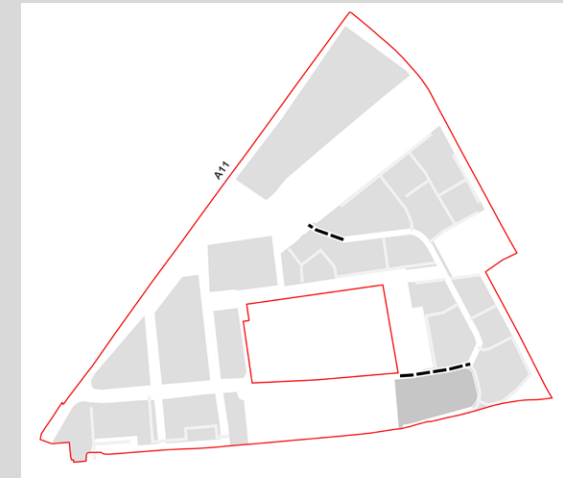
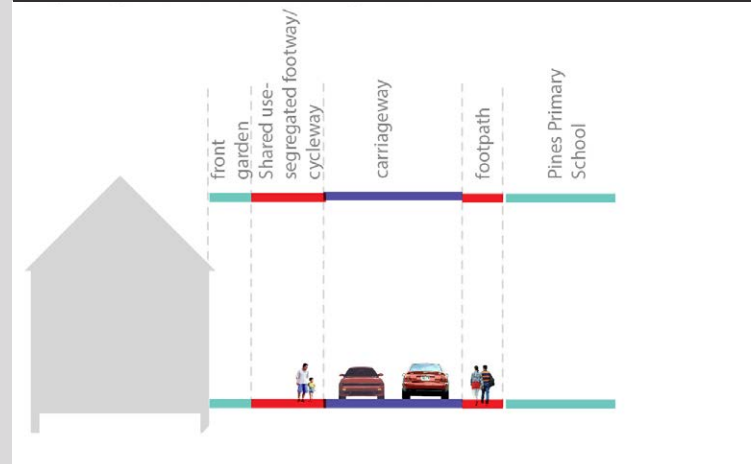


FIGURE 31 ILLUSTRATIVE SECONDARY CARRIAGEWAY STREET SECTION



Must have a design speed no greater than 20mph achieved by suitable layout design rather than physical measures such as ramps.

Carriageway must be at least 5.5m wide

Tertiary Road

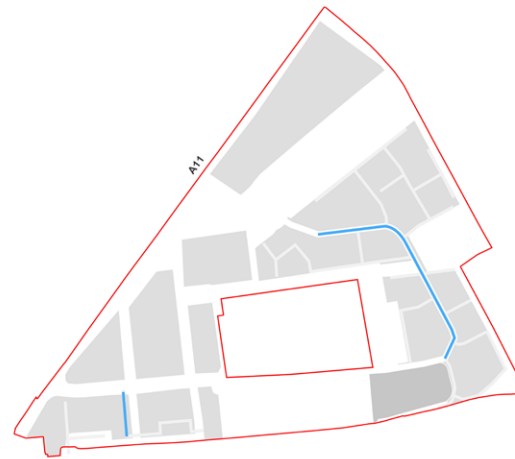
The Tertiary Road provides the key vehicular route through the site as well as pedestrian and cycle access. It forms the central part of the spine road and has an important movement function. It should therefore be characterised by distinctive tree planting on one side. It should have a consistent formal character reinforced by tree planting and should be punctuated by a series of distinctive spaces along its length to create an interesting and varied townscape within the development.

There should be direct access for vehicles to individual properties fronting onto the street.

Tree planting within a landscaped verge should typify this street typology. Typically, there should be a 6m verge on one side of the road with a segregated footpath/cycleway wide footpath/cycleway and a footpath on the other side of the road.

Materials should be selected that relate to the urban location, including lighting, kerbs and paving finishes. Statutory services should be accommodated beneath the footpaths either side of the carriageway; they should not be in verges or in other locations that would prevent the planting of trees along the road. Details should be designed in conjunction with Suffolk Design Street Guide.

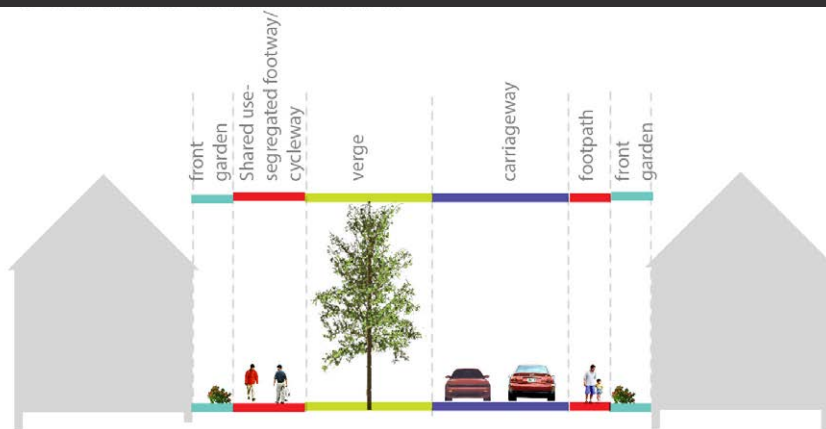
FIGURE 32 INDICATIVE LOCATION PLAN



Design Examples



FIGURE 33 ILLUSTRATIVE TERTIARY CARRIAGEWAY STREET SECTION



Should have planting and parallel parking in order to provide traffic calming, but should not be longer than 3 continuous bays

Must provide footways on any side that borders dwellings

Must have a design speed no greater than 20mph achieved by suitable layout design rather than physical measures such as ramps.

Carriageway must be at least 5.5m wide



2m front garden

3.5m apron/footpath

4.0m verge

flowers

6.5m road

2m front garden
2m footpath

Side Streets

Side Streets have a strong priority of pedestrians over vehicles with an informal character accommodating some on street parking with planting. The road width should vary with a minimum of 5.5m. Streets should occasionally form pinch points in order to calm traffic and emphasise key spaces. The road should be cyclist and pedestrian friendly. Contrasting surfaces can be used to emphasise that carriageway is shared by vehicles and pedestrians

Tree planting where space allows would provide a softening effect.

The Side Street can also provide a choice of safe and convenient routes for pedestrian and cyclists.

Planting should be restricted to occasional street trees where space allows between parking bays in the public realm. Consistent set-backs and front garden widths should provide some opportunities for soft landscaping within the private realm to increase landscaping in the public realm. Statutory services should be accommodated beneath the road. The design of the streets in terms of layout and detailing should encourage low traffic speeds.

All details must be designed in conjunction with Suffolk Design Street Guide.



FIGURE 34 INDICATIVE LOCATION PLAN

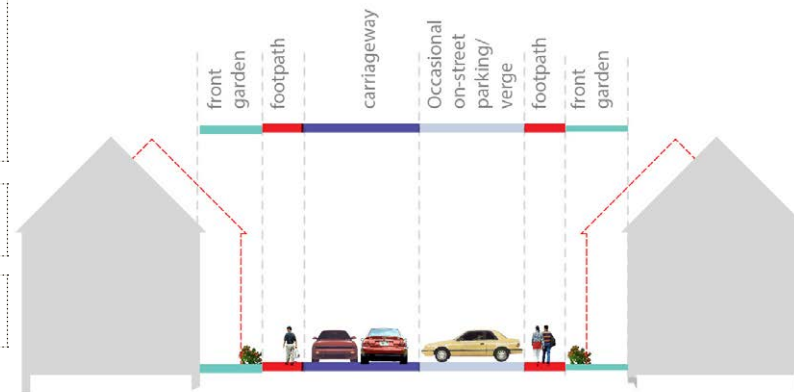


FIGURE 35 ILLUSTRATIVE SIDE STREET SECTION

Should have planting and parallel parking in order to provide traffic calming, but should not be longer than 3 continuous bays

Must provide footways on any side that borders dwellings

Carriageway must be at least 5.5m wide



Must have a design speed no greater than 20mph achieved by suitable layout design rather than physical measures such as ramps.

Should give direct driveway access

Must provide footways on any side that borders dwellings

Should encourage a permeable grid structure. Should include trees and grass verges when appropriate

Green Street

The Green Street is a shared surface street forming the visual and pedestrian connection to the open space located north of the development. Because of its function as an important linkage, there should be a minimum of 6m verge on one side. It has a strategic importance connecting two main green spaces together. Green Street should have direct access for vehicles to individual properties as well as accommodating street parking.

It should have soft landscaping which should typically take the form of tree groups and planting occasionally interspersed with parking bays. Statutory services should be accommodated beneath footpaths.

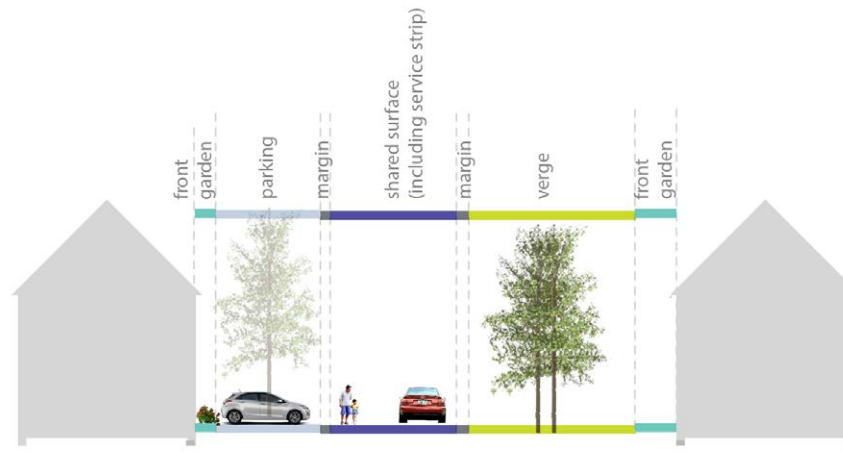


VIEW 2 - Green Street

FIGURE 36 INDICATIVE LOCATION PLAN



FIGURE 37 ILLUSTRATIVE GREEN STREET SECTION



Must have a design speed no greater than 15mph

The road should be a minimum of 5.5m

Should have a maintenance strips of min 0.5m

Can include on street parking where this is not obstructive to cyclists and pedestrians

Lanes and Private Drives

The Lane should accommodate a low level of low speed traffic providing access to individual dwellings. Opportunities should be taken to use flush surfaces and textured treatments to enhance a 'shared surface' approach to Green Lane where possible.

The Lane should have an organic and informal rural character with building frontage on one side and open space on the other side. Buildings should set irregularly with changing setbacks and building line. Variations in setbacks should provide some opportunities for soft landscaping to the street.

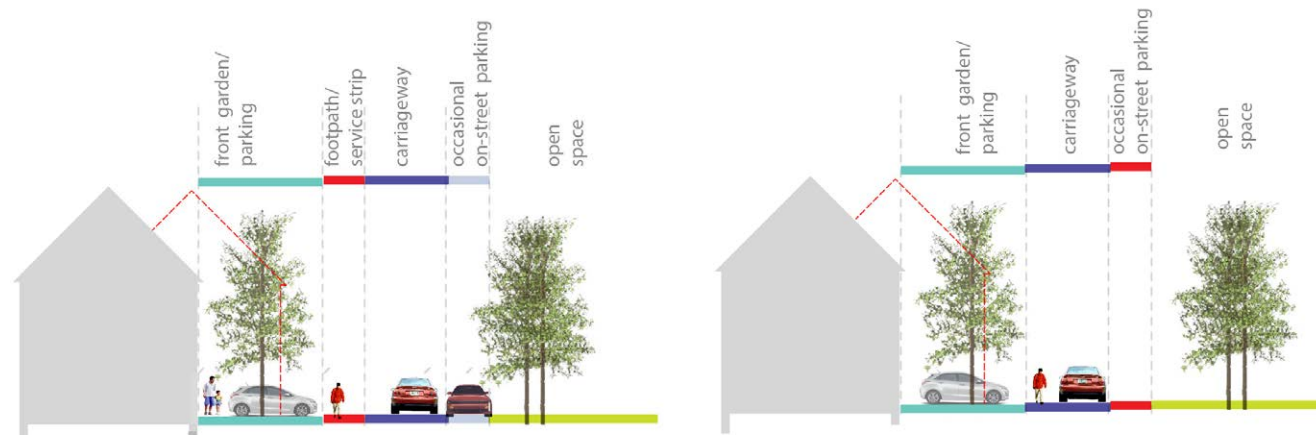
Private drives are shared by private cars and pedestrians only providing access to frontages along green spaces and parks. Traffic flows should be low enabling a mixing of pedestrians and vehicles.

Soft landscape such as small trees, hedging and shrubs are a major feature. Services should be located under the road surface.



FIGURE 39 ILLUSTRATIVE LANE AND PRIVATE DRIVE SECTION

FIGURE 38 INDICATIVE LOCATION PLAN



Mews

Mews should generally be more informal in character and should encourage walking and cycling. They should have a quieter and pedestrian friendly character, human in scale, and enclosed by building frontages on both sides that would create a safe environment for pedestrian, children and cyclists.

The street should accommodate soft landscaping which should typically take the form of tree groups and planting, occasionally interspersed with parking bays located along the street. Statutory services should be accommodated beneath the side of the surface.

Surface treatment of mews could potentially have shared-surface quality in some areas to provide a pedestrian-safe environment and quality planting to enhance the character of streetscape.

Design Examples



FIGURE 40 INDICATIVE LOCATION PLAN



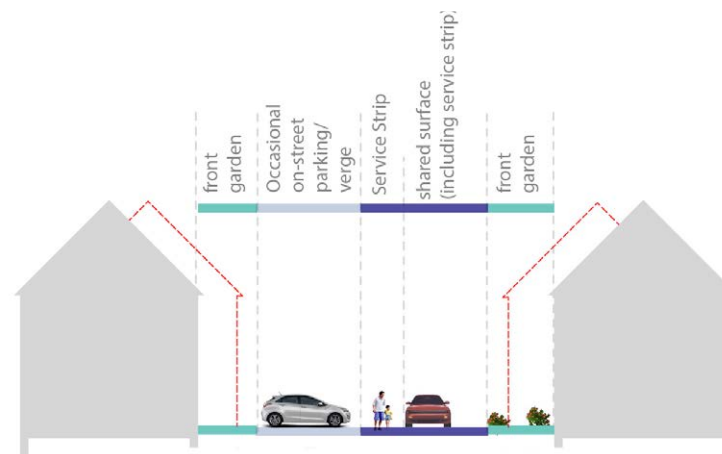
Must have a design speed no greater than 15mph

The road should be a minimum of 5.5m

Should have a maintenance strips of min 0.5m

Can include on street parking where this is not obstructive to cyclists and pedestrians

FIGURE 41 INDICATIVE MEWS SECTION



BLUE INFRASTRUCTURE

Sustainable Drainage

SALP Policy SA10(G) requires the new development to have regard to the proper functioning of the existing SuDS infrastructure located on the site.

Published geological records for the area indicate the site has the following geology:

- Superficial Geology: River Terrace Deposits – Sand and Gravel
- Base rock Geology: Holywell Nodular Chalk Formation and New Pit Chalk Formation (undifferentiated) – Chalk.

The nearest watercourse is the River Kennett located approximately 1.5km south west of the site. The site is located in Flood Zone 1 and there are no areas within the site that are at risk from flooding.

The majority of site is within a Zone 3 Groundwater Source Protection Zone.

Surface Water Drainage

The surface water drainage strategy will be designed in accordance with Suffolk County Council's SuDS Guidance, unless there are reasons to depart from it.

Infiltration testing has confirmed that the site is suitable for the use of infiltration systems and soakaways. The strategy will be designed for:

- a 1 in 100 year return period;
- 20% climate change uplift; and
- 40% sensitivity check for climate change.

The approximate volume of attenuation storage required on site has been determined using infiltration storage systems in Microdrainage as follows:

- Residential development (300 dwellings): 720m³
- Employment: 840m³
- Local centre: 75m³

This may be subject to review at the detailed design stage.

A combination of SuDS including infiltration measures will be used to discharge surface water:

- **Roads:** all roads, excluding private drives, will be designed so that surface water discharges to ground. Suitable measures will be provided to ensure that runoff is appropriately treated in line with the SuDS Manual.

- **Private shared surfaces and driveways:** will use swales or permeable paving to provide an adequate level of treatment in line with the SuDS Manual.
- **Roof runoff:** water run-off from domestic roofs and commercial buildings will discharge to private soakaways. Domestic soakaways will be in rear gardens. Consideration will be given to the use of water butts and rainwater harvesting.

The SuDS features will be designed to integrate with the landscape and provide both habitat and amenity value. The use of SuDS basins will provide an opportunity for grass or wildflower planting and provide wildlife value.

Land required to provide SuDS will not form part of the developments public open space provision. It will be integrated with adjoining areas of open space and landscaping and not act as a barrier to public access into the linear parks.

The existing SuDS ditch within the area identified for the expansion of the primary school will be relocated outside the school site as part of the surface drainage strategy for the whole site and discussion will be required with Suffolk County Council as the Lead Local Flood Authority and the education authority to ensure the needs of both services are met.

• Precedence of SUDs basin



LANDSCAPE AND ECOLOGY

Landscape

SALP Policy SA10 (C) and (D) requires measures to ensure the management of Breck grassland and the provision of strategic landscaping and open space, as well as the retentions of tree belts. A landscape buffer is also required along the A11 (SA10(E)).

Landscape Strategy

The landscape principles for the development of this site have been set out under the Masterplan Design Principles earlier in this document and should be referred to in developing a detailed strategy as part of any planning application.

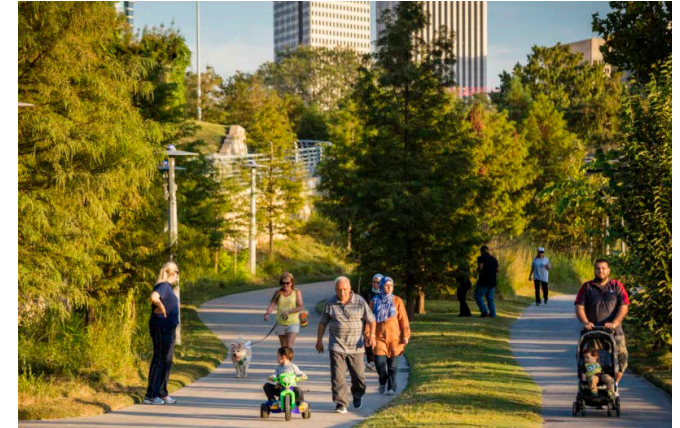
The landscape strategy should aim to assimilate residential and industrial development into the landscape while providing recreational opportunities as an alternative to those within the Breckland SPA.

The design should remain local in character so as to not to create a destination for passers by on the A11.

Development will be set within a landscape framework informed by the characteristics of the 'Estate Sandlands'.

The strategy will conserve and protect the distinctive lines of pine trees, re-connecting the fragmented habitats through the creation of accessible green corridors between open spaces and the wider landscape structure.






The ecological function of the landscape will be improved through Green Infrastructure provision and public accessibility will be increased.



Landscape Vision

FIGURE 42 LANDSCAPE MASTERPLAN

1. New and Enhanced Landscape Buffer
2. New and Enhanced Woodland Edges
3. Pine Park
4. Beech Belt Park
5. Green Corridors
6. Green Street

-  Existing Woodland Belt Outside Estate Control
-  Local Centre
-  Employment
-  Residential
-  Existing Farm Access to be Retained



Suitable Accessible Natural Green Space (SANG)

SALP Policy SA10 (B) requires the development to provide measures for influencing recreation in the surrounding area, to avoid a damaging increase in visitors to Breckland SPA and an increase in recreational activity in adjacent farmland. Measures should include the provision of suitable alternative natural greenspace which is well connected and the enhancement, and promotion of dog friendly access routes in the immediate vicinity of the development, barriers to access and/or other agreed measures.

The Accessible Natural Greenspace Study that forms part of the evidence base for the SALP reviews accessible natural greenspace provision at the District's main settlements, explores the opportunities for new greenspace and access routes that could be delivered to support the planned growth, and outlines a recreation pressure mitigation strategy for each main settlement. In relation to Red Lodge, the strategy focusses on the provision of linked walking/cycle routes around the settlement boundaries and towards the River Kennett via existing Public Rights of Way.

The Study considers the Natural England SANG design criteria in relation to the district itself, and notes that while the points in the Natural England guidance give all the elements of an excellent SANG, within the District much of the area is designated. It therefore concludes that the approach within the District should be to increase greenspace and green networks in a flexible way, for example by providing smaller areas of greenspace where space does not allow but ensuring it is connected to other greenspace by attractive walking and cycling routes. It goes on to set out key features which should be prioritised for SANG design which are adapted from the Natural England guidance and made relevant to the District (acknowledging smaller areas will not be able to accommodate all these). The SANG strategy for this site will have regard to this.

SANG Concept Design

The aim of the SANG strategy is to design, create and manage a SANG which will function to deflect visitors away from Breckland SPA, so as to ensure the proposals do not adversely affect the integrity of the designation. The target user group of this SANG is those who might otherwise use the SPA for the purposes of daily dog walking and other recreational activities.

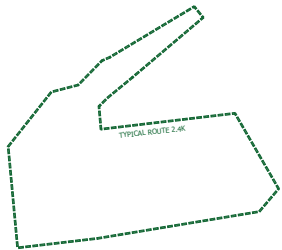
Following approval of the masterplan by the Council, this strategy will be further developed so as to produce a SANG Concept Design. The Concept Design will be submitted with any planning application in accordance with Policy SA10.



SANG Key Principles

The SANG will include the key principles identified below:

- A variety of circular walks within the site and beyond with new walking routes connecting to the two new parks and wider footpath network within Red Lodge;
- Provision of appropriate access for visitors;
- Provision of a safe access on foot for users;
- Paths must be easily used and well maintained with a mix of surfaced and unsurfaced finishes to avoid the site being too urban in feel;
- SANG must be perceived as semi-natural spaces, with intrusion of artificial structures limited to areas close to developed edges except for the essential infrastructure such as way-markers and benches;
- Access within the SANG must be largely unrestricted with plenty of space provided where it is possible for dogs to exercise freely and safely off lead;
- SANG should be clearly sign-posted or advertised in some way. This should be considered at the detailed design stage;
- Although not a larger SANG, the Landscape Strategy should incorporate information boards setting out the layout of the SANG and walking routes as entrance features;
- Although not a larger SANG, a variety of habitat types will be provided;
- Clearly sign post or advertise SANG, including information boards about walking routes;
- Provision of public art within the open space.



CIRCULAR WALKING ROUTE



Public Open Space

Open space is to be provided in accordance with Forest Heath District Council's Supplementary Planning Document for Open Space, Sport and Recreation Facilities, October 2011 or any replacement document. The open space calculator within the SPD indicates that 1110 occupants will arise from the proposed mix of 300 dwellings with a requirement of 36sqm of open space per person.

Two new parks are proposed. Their design will take account of the key SANG principles and the additional design principles outlined for each below.

Pines Park: Key Principles

In the north of the site, an area of accessible multi-functional open space will be created with informal trails, various habitats and more intimate spaces amongst the vegetated structure of the edges.

Sensitive planting will enable these accessible landscape features to integrate into the open space while also providing visual and acoustic screening.

The requirement for acoustic attenuation from the audible intrusion of traffic using the A11 will be addressed through the creation of naturalised mounding in the northern section of the park.

The centre of the Park should be kept largely open to provide space to exercise dogs, with pockets of tree and shrub planting with Scots pine to replicate the characteristic 'pine lines' of the Estate Sandlands Landscape Character Type.



The creation of a woodland buffer with a woodland edge and wildflower meadow will create a robust edge to the Park and in addition, create an enhanced habitat for wildlife.

Provision of footpaths across the park will create an accessible space that can be enjoyed by all. They will include a mix of surfaced and unsurfaced finishes.

Beech Belt Park: Key Principles

A smaller, more intimate space than Pines Park, Beech Belt Park would be focused around play.

The provision of formal play equipment would be set amongst indigenous planting that would create a transition between the woodland belts on the eastern edge of the site and the residential areas within it.

Planting in Beech Belt Park would be more sparse than the adjacent woodlands, replicating the historic landscape characteristic of 'fields with trees'.

Accessible green corridors would connect Beech Park to the larger and more open space to the north, as well as creating a connection to Davids Belt woodland edge to the south.

This would contribute to the creation of a holistic and landscape scale framework of environmental improvements to deliver a sense of place to the site.

Site Boundaries

Site boundaries to the south and east are sensitive. Any perimeter boundary should take account of its setting.



Acoustic Strategy: A11 Traffic Noise

The acoustic strategy must work with the landscape to mitigate noise in a natural way and minimise the use of acoustic structures.

A noise survey has been undertaken and identified the location and extent of any acoustic mitigation as shown on Figure 44.

Along the A11, closest to the noise source there shall be an acoustic screen and the existing bund shall be retained. To the north of Pines Park there shall be a series of earth mounds. The bund and earth mounds will be planted with and the acoustic screen shall be landscaped with a mix of native woodland shrubs, trees and grasses.

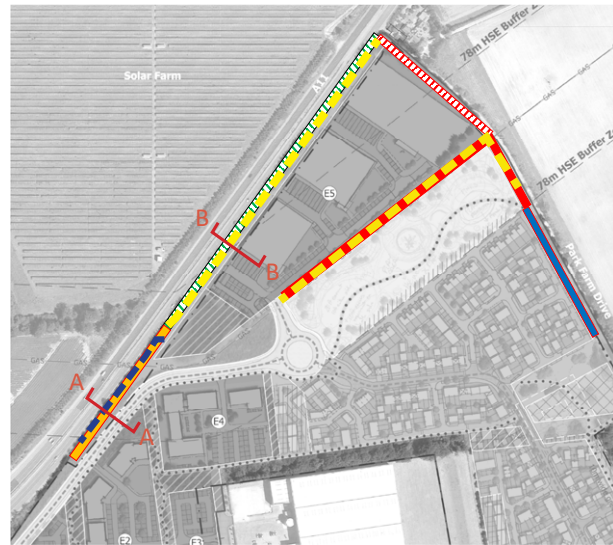
The acoustic screen should not encroach into existing root protection areas. Opportunities will be considered for planting the outside face (fronting the A11) of any acoustic fence.

Attention must be given to the relationship between the existing bund and new acoustic screen to ensure that there is no gap in acoustic provision.



FIGURE 44 ACOUSTIC CONSULTANTS RECOMMENDATIONS

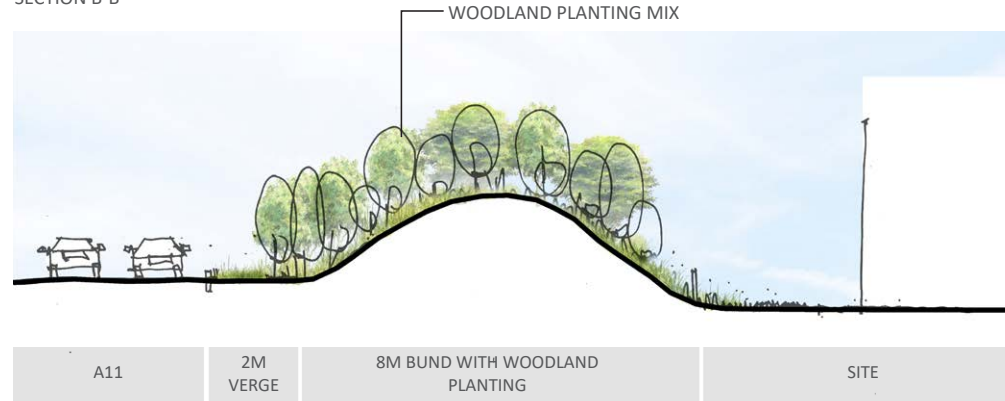
Recommended acoustic barrier extent



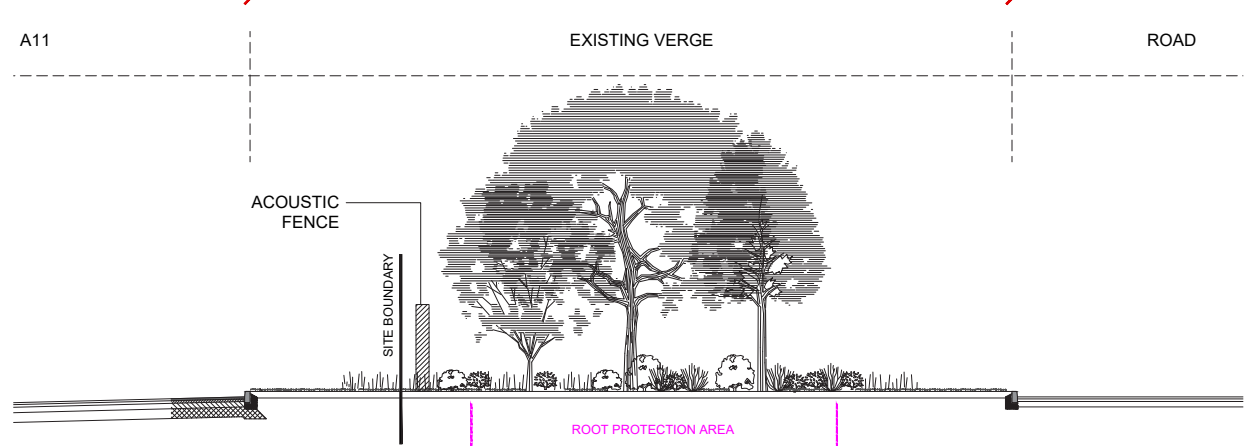
Key

Existing bund (approx. 2m high)	
Proposed 2m bund (Landscaped with POS)	
Proposed 2m bund or acoustic fence	
Proposed 2m bund or acoustic fence (if no shielding building)	
Residential protection from façade design and local fencing	
Acoustic Bund	
Acoustic Fence	

SECTION B-B



SECTION A-A



• Precedence of Woodland Grass Edge



• Precedence of close boarded fence

Green Corridors: Key Principles

Green corridors include the two existing pine lines which are to be retained and enhanced; the Green Street located north of the development forming a linkage between the Pine Park and the Beech Belt Park; and a new tree line to be created between the employment/residential area to the north west of Hamelin (Figure 42).

Connected and accessible green corridors throughout the site will increase the recreational and ecological function of the proposed development and reinforce the sense of place.

Existing trees will be maintained and a sensitive approach to pedestrian cycle path construction will be used to ensure their retention and protection. This will include avoiding RPA's where possible and the use of No Dig construction to avoid root damage.

Where canopy openings allow pockets of wildflower grasses will create small areas of respite along the route, providing wildlife and amenity value.

A sensitive approach to lighting is required to maintain the dark corridor routes shown on Figure 5 for bats.



Woodland Edges: Key Principles

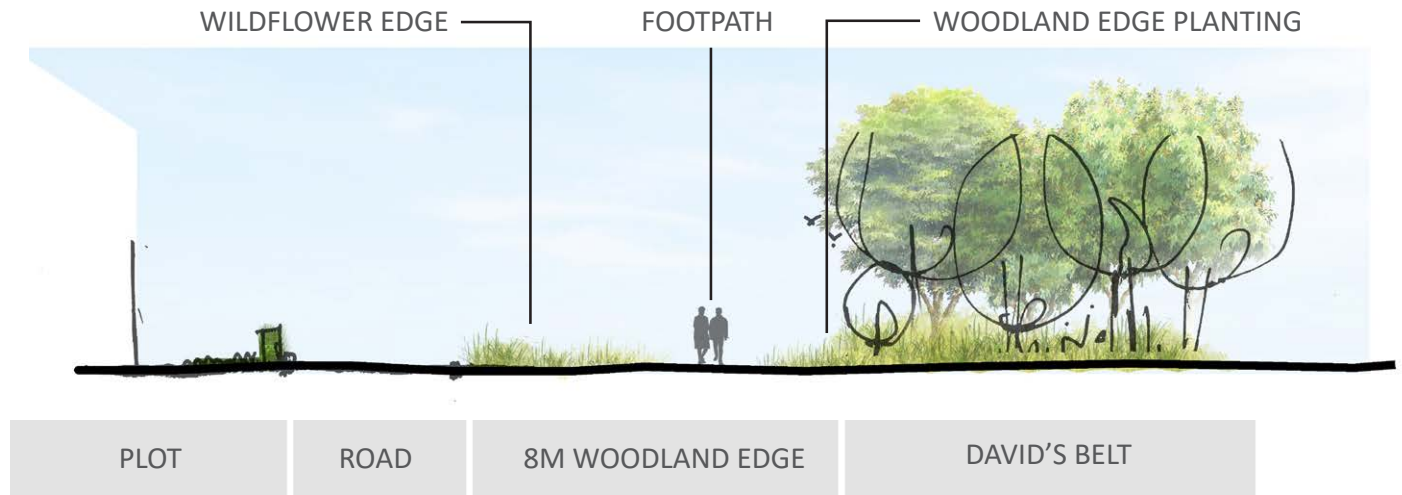
The Woodland Edge is located to the west of David's Belt.

The landscape design should create a sensitive woodland edge character that enhances the character of David's Belt whilst softening the edge of the residential development.

The boundaries of the woodland will be treated with a woodland edge typology creating a slope of canopy and shrubs with shade tolerant grassland habitat.

Footpaths along this buffer, connect the neighbourhoods north to south and provide an attractive recreational route along the woodland edge.

FIGURE 45 WOODLAND EDGE : SECTION



• Precedence of woodland edge



• Precedence of Woodland Edge



• Precedence of Woodland Grass Edge

Ecology

An Ecological Appraisal has been produced which sets out the results of the surveys carried out, and assesses the development parameters. Survey work was carried out in 2016, and updated in 2018 and 2020. This has allowed the ecological features of the site to be identified so that key areas can be retained and protected within the masterplan.

The site itself is not subject to any statutory or non-statutory ecological designations. Potential effects on European designations are considered in a separate document to inform a Habitats Regulations Assessment in accordance with SALP Policy SA10 B). The nearest statutory and non-statutory designations are Red Lodge Heath SSSI and Worlington Chalk Pit CWS. No effects on these designations are anticipated from the proposed development.

There is a commitment to delivering net gains for biodiversity in accordance with legislative/ planning policy requirements, through a mix of on and off site biodiversity measures. The details will form part of a planning application.

The site comprises habitats of low/moderate ecological value, dominated by arable land, with woodland belts and tree lines, grassland, tall ruderal vegetation and scrub. The masterplanning exercise has sought to avoid habitats losses as far as possible, and therefore proposed development is focused on areas of intensive arable land which has minimal ecological value. Some habitat loss will occur, but this is not considered to be significant and will be compensated for by the creation of green space and landscape planting. Losses of trees are limited to a small number of trees within Lower Beech Belt for the main spine road.

The masterplan site is utilised to some degree by an assemblage of eight bat species with a small number of roosting opportunities in trees. Badger is known to be present within the wider area, but no evidence of presence was found within the site. Surveys in 2016 and 2020 found no evidence of reptiles. Breeding bird surveys recorded a modest assemblage of 20 species with a small number of farmland species such as Yellowhammer. In terms of fauna, generally the application site supports populations of low ecological value at the site or local level. Due to the bat species recorded, the site is considered to be of moderate value at the district level for commuting/foraging bats. The masterplanning exercise has sought to retain habitats associated with the fauna recorded on site as far as possible, such as the woodland and tree belts.

The following preliminary measures have been identified to mitigate the impact of the development, which will be further developed and refined as the proposals are brought forward (measures in relation to Breckland SPA are set out in the Document to Inform an HRA):

- General environmental best practice, such as pollution prevention measures and use of tree protection fencing;
- Any trees that require removal and which are identified as having low bat roosting potential will be soft felled;
- Where removal of part of any linear feature is required, vegetated hop-overs should be incorporated into the detail design such that any gaps will be no more than 15m wide;
- Provision of dark corridors for nocturnal wildlife (as shown on Figure 5);
- Pre-construction checking surveys for Badger and Hedgehog, and safeguard measures during construction;
- The lighting scheme should avoid illuminating any commuting/foraging linear features;

- Mitigation for any vegetation removal carried out during the nesting bird season;
- New planting should comprise locally native species;
- Target faunal enhancements such as provision of bat/bird boxes (integrated to new dwellings and on suitable trees),
- The provision of any barrier to human access to adjacent farmland for recreation must not be a barrier to wildlife. Its design must incorporate measures to minimise wildlife collision risk and ensure access for small animals (i.e. hedgehogs) and take account of any impact on trees.

TRANSLOCATED GRASSLAND

The wildlife audit (www.westsuffolk.gov.uk/localplanevidence) identified that the grassland flora within the sustainable drainage channel (RL21) was quite herb-rich.

Potential for existing valuable grassland within Suds on site to be Translocated to proposed drainage basins





OTHER POTENTIAL OPPORTUNITIES AND BENEFITS

Established urban design principles have informed this masterplan to ensure that the proposed mixed use development to the north of Red Lodge will have a strong sense of place, that it retains and enhances landscape features, is legible and permeable, with good provision for vehicular, pedestrian and cyclist movement through the site. It has been informed by technical baseline studies, landscape and ecology assessments and discussions with consultees. Together these have highlighted opportunities to enhance green infrastructure, biodiversity, the connectivity of green spaces within and beyond the site, to improve health and wellbeing with a greater range of different recreational experiences and to provide a through vehicular route to the south.

Other Potential Opportunities and Benefits

Vehicular Through Route

In its commentary on site SA9(b), Land East of Red Lodge: North, the SALP states that emergency access and cycle and pedestrian links should be created to the proposed mixed use site directly to the north, that is site SA10(a). This requirement to provide an emergency access is noted in the section relating to Access and Movement.

In the context of promoting accessible and permeable development, potential has been identified to create a vehicular through route serving the two allocated sites SA9(b) and SA10(a) respectively. This would provide a second point of access for site SA9(b) and an additional access for site SA10 providing greater flexibility and opportunity to distribute traffic movements.

Any planning application which includes measures beyond the requirements set out in SALP Policy SA10 would need to justify their inclusion and evidence that they are achievable.

FIGURE 46 FURTHER POTENTIAL OPPORTUNITIES PLAN



IMPLEMENTATION FRAMEWORK

The new neighbourhood at North Red Lodge will be developed over a number of years. All of the land necessary to deliver the allocation SA10(a) is controlled by the Estate and there are no issues regarding land ownership or assembly.

None of the utility providers have indicated that there are any concerns providing services to the site.

Housing

It is anticipated that an outline planning application will be submitted with all matters reserved except access. Once planning permission has been granted, further applications to discharge reserved matters and conditions will be submitted either by the Estate and/or by the developer.

If a single developer was building the development, the average annual completion rate is expected to be circa 50 dwellings and the development of around 300 dwellings is estimated to take 6.5 years. A phasing plan will be submitted with the reserved matters application.

Employment and Retail

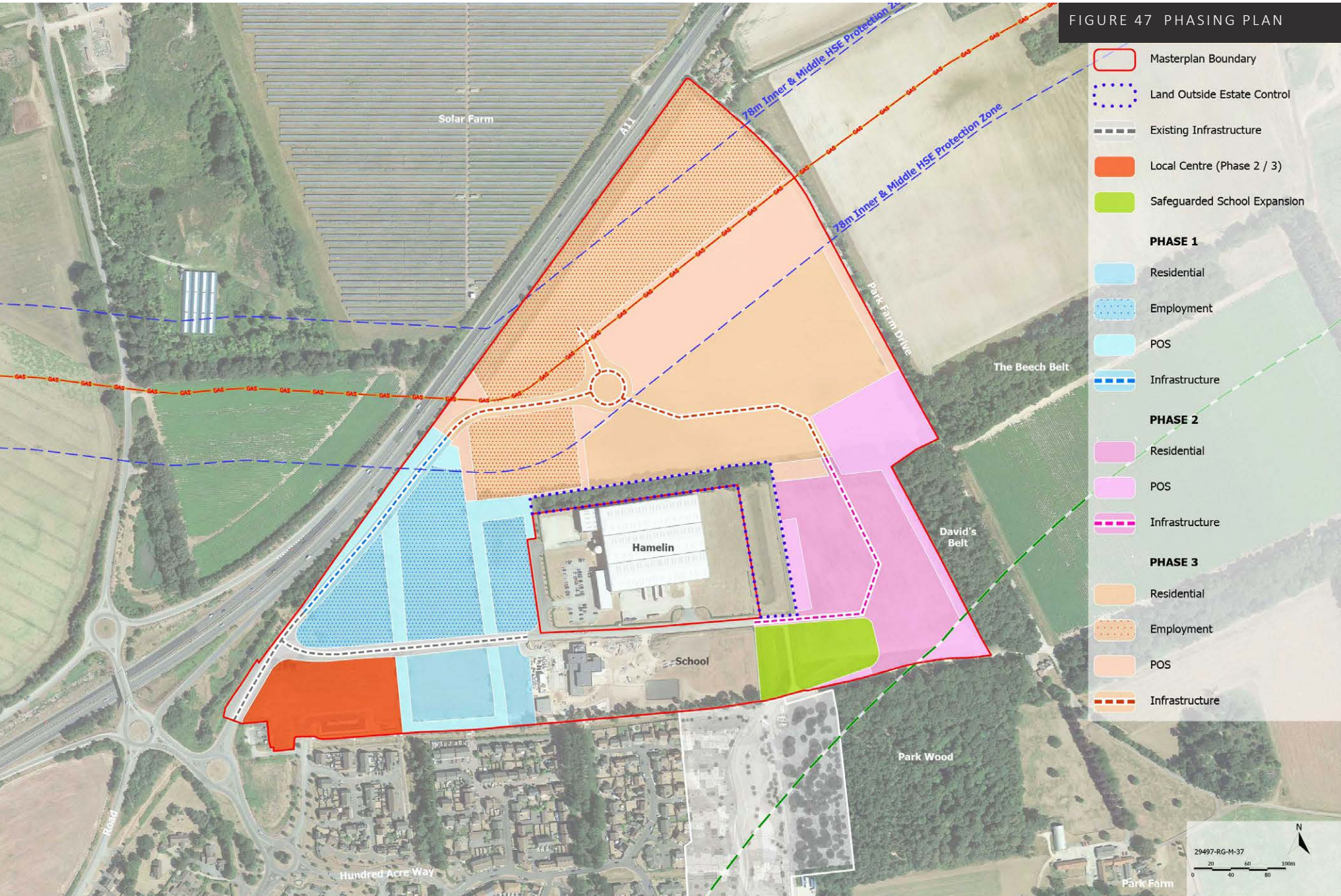
Market interest has already been expressed in both the employment land and retail floorspace. Once planning permission has been granted this interest will be followed up and the sites will be marketed. The rates at which the employment land is developed will be dependent on market conditions at that time.

Infrastructure Provision and Phasing

Delivery of infrastructure will take place in the context of planning applications and will be secured through the use of planning conditions and planning obligations.

Delivery of the Red Lodge North development as a whole will be phased as shown on the Phasing Plan (Figure 47).

FIGURE 47 PHASING PLAN



- Masterplan Boundary
- Land Outside Estate Control
- Existing Infrastructure
- Local Centre (Phase 2 / 3)
- Safeguarded School Expansion
- PHASE 1**
- Residential
- Employment
- POS
- Infrastructure
- PHASE 2**
- Residential
- POS
- Infrastructure
- PHASE 3**
- Residential
- Employment
- POS
- Infrastructure

